

Borough of River Edge, New Jersey

NEW BRIDGE LANDING STATION REDEVELOPMENT PLAN

The original of this Plan was signed and sealed
in accordance with N.J.S.A. 45:14-12

Prepared for:

The Borough of River Edge



Prepared: June 12, 2006

Revised: June 8, 2007

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INTRODUCTION

This Redevelopment Plan is prepared at the direction of the Borough of River Edge Mayor and Council in cooperation with the Borough of River Edge Planning Board.

A Redevelopment Plan is a work in progress. It is a guide which serves to provide a framework for land use and development within a redevelopment area; and also serves as an opportunity to encourage the residents, business owners and property owners to improve and renovate certain parcels of land individually or through public-private agreements, redeveloper agreements and other entities.

The designation of a redevelopment area and the adoption of this Plan provides an opportunity to provide much needed property tax relief as well as public and private improvements to encourage the highest and best use of this area and invigorate growth in the area.

This Redevelopment Plan serves as a tool to guide the revitalization of the New Bridge Landing Station Redevelopment Area. While the Borough Council may approve the initial Plan, the residents, businesses and property owners of River Edge must always have the opportunity to apply to modify and improve this Redevelopment Plan to achieve the expresses and desired goals. A good plan will be able to remain relevant with the times. That is possible only with the consistent input of the people of the Borough of River Edge. It is also critical to the successful implementation of this Plan to reach out to our community commercial property owners, with the sensitive and judicious use of redevelopment tools at the Borough's disposal.

It is also the policy of the Borough of River Edge that property owners and businesses located within the Redevelopment Area will be given every opportunity to participate in the redevelopment program through redeveloping their properties in accordance with the land uses, building and design requirements of this Plan.

DESCRIPTION OF THE PLAN

SECTION 1.0 DESCRIPTION OF THE PLAN

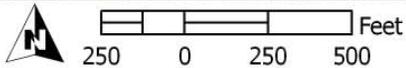
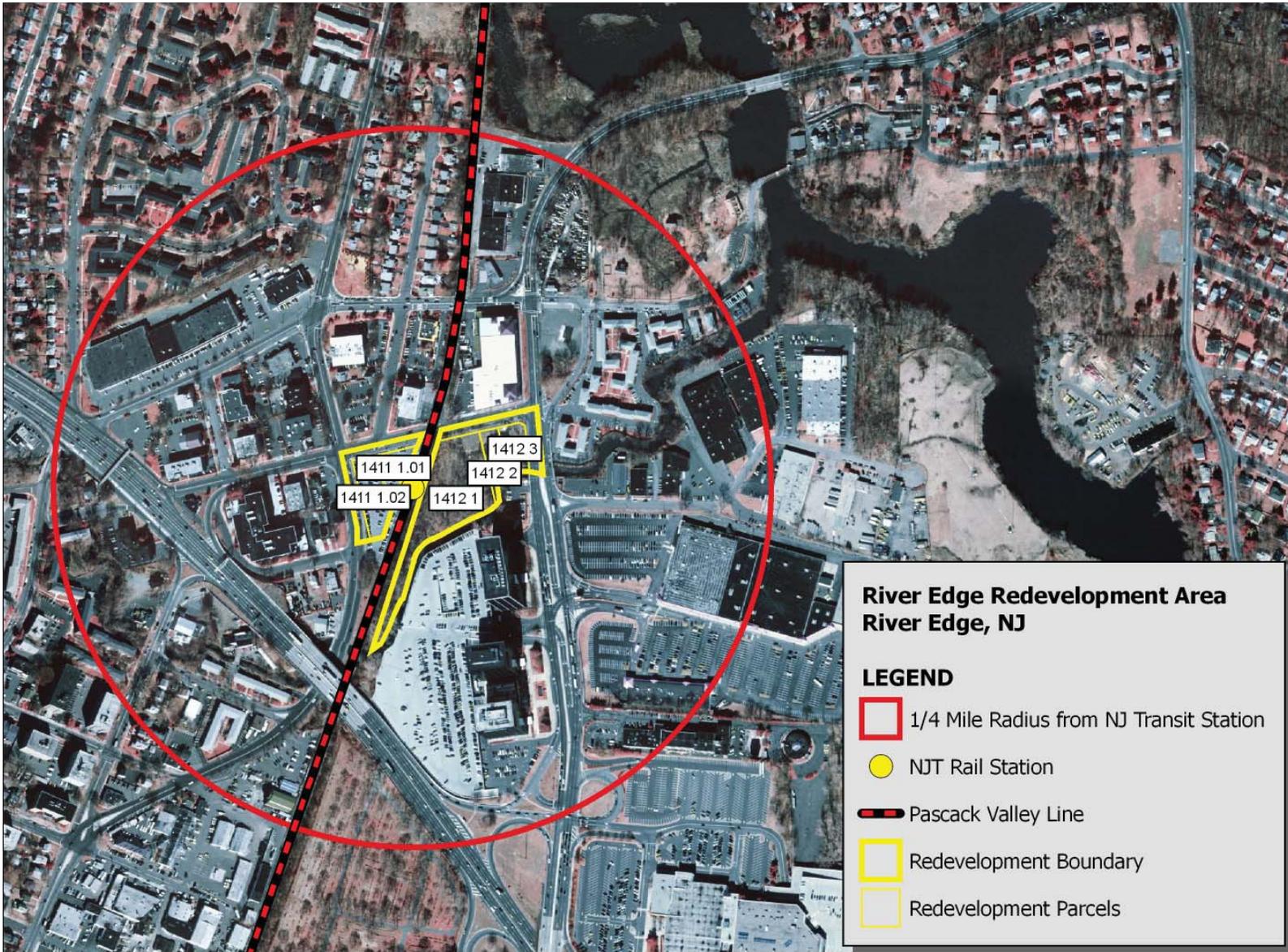
The New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area is comprised of approximately 3.8 acres net (excluding all rights of way). The gross area for the New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area including ROW's is 4.94 acres. The Redevelopment Boundary of the site is shown on **Exhibit 1.0, New Bridge Landing Station - Aerial Photo** and **Exhibit 2.0, Lot and Block Map**. The Redevelopment Parcels comprising the 3.8 acres are shown in the lighter yellow lines.

The study area consists of the following tax blocks and lots:

Block 1411 (Lots 1.01, 1.02)

Block 1412 (Lots 1, 2, and 3)

These parcels shall be reorganized into two redevelopment parcels, hereafter labeled A and B on **Exhibit 3.0, Development Parcels**. This exhibit shows the shape and location of each redevelopment parcel, which shall be hereafter referred to as parcels: A, that is the area on the southwest corner of Grand Ave and Kinderkamack Rd; and B, on the southeast corner of Grand Ave and Hackensack Ave. The New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area will hereafter be referred to as the "Area" or the New Bridge Landing Station Redevelopment Area.



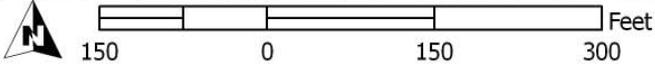
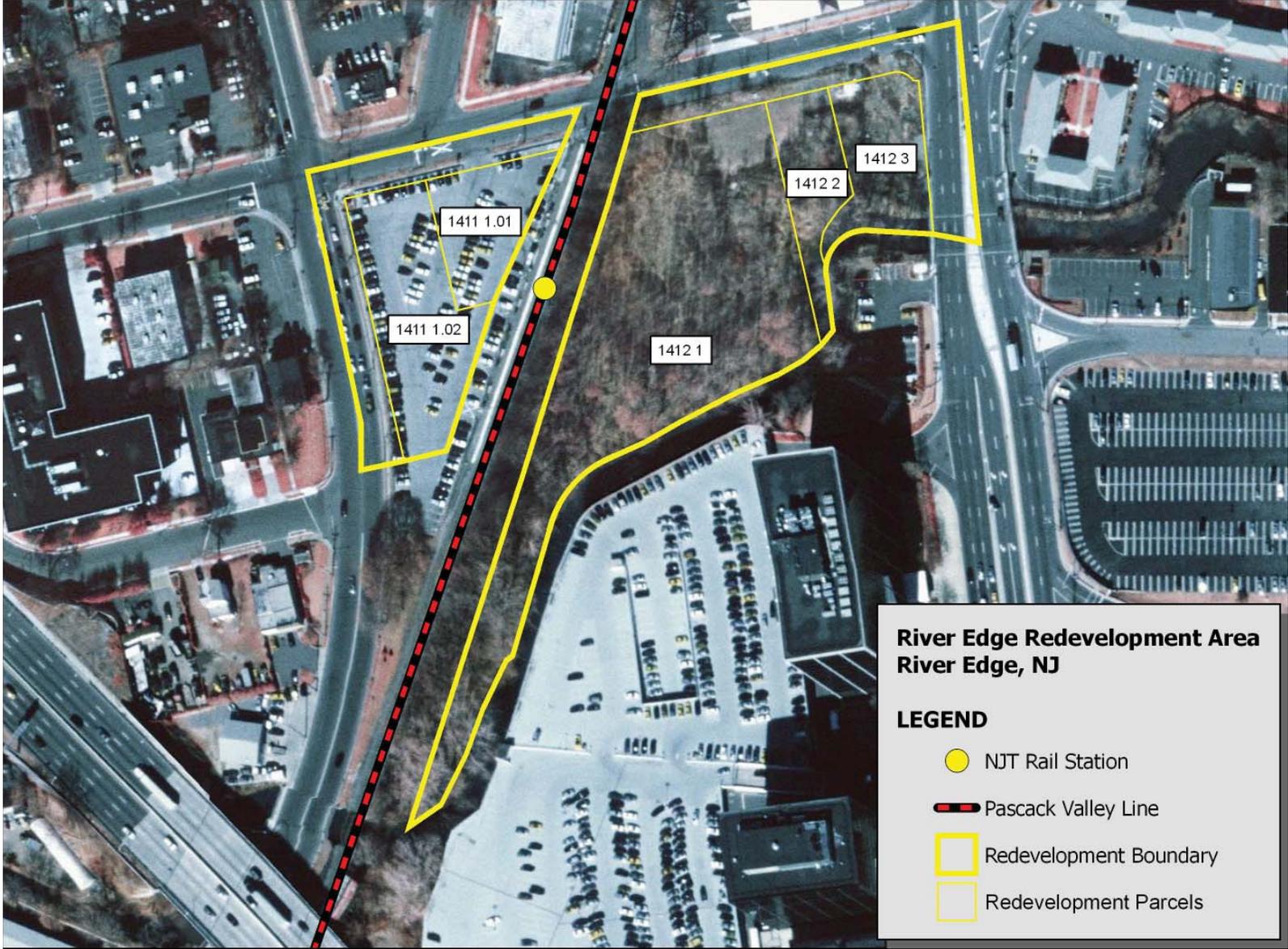
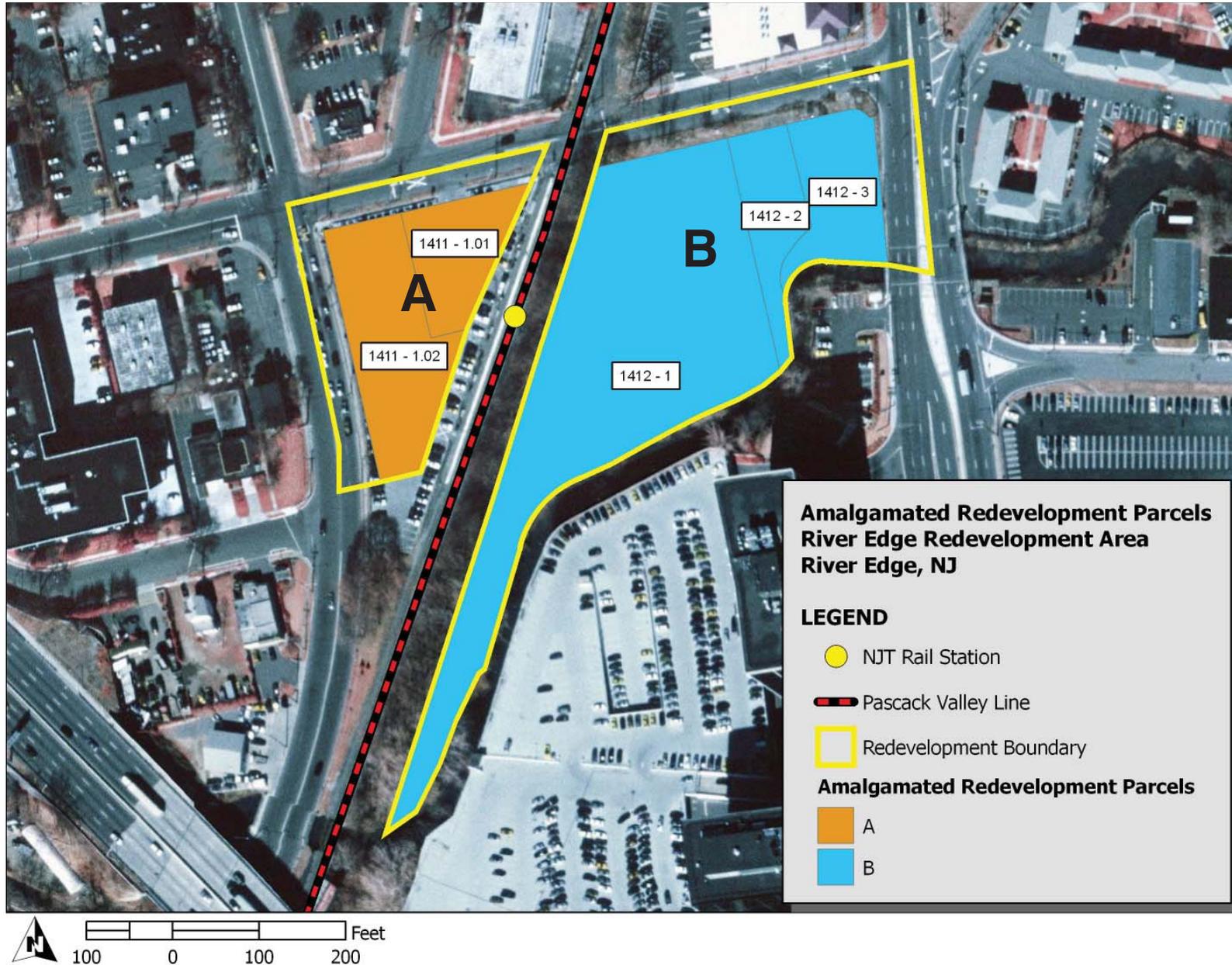


Exhibit 3.0 Amalgamated Redevelopment Parcels



STATUTORY BASIS OF THE PLAN

SECTION 2.0 STATUTORY BASIS OF THE PLAN

The Plan has been prepared in furtherance of the Borough of River Edge Council Resolution #05-123 determination on March 7, 2005, that the area meets the statutory criteria for designating an “area in need of redevelopment” pursuant to the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et seq. (the “LRHL”). The Borough of River Edge Council subsequently directed its consulting planners, A. Nelessen Associates, Inc., to prepare a Redevelopment Plan. The consultants submitted their plan, entitled *New Bridge Landing Station Redevelopment Plan*, to the Borough of River Edge in June, 2006. For this Plan to become effective, it must be adopted by the Mayor & Council by Ordinance pursuant to N.J.S.A. 40A:12A-7.

PURPOSE OF THE PLAN

SECTION 3.0 PURPOSE OF THE PLAN

The purpose of this Plan is to set forth the terms and conditions under which the New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area may be redeveloped by a designated Redeveloper(s). The Plan describes, among other regulations, the basic townscape requirements and recommendations. The Building Regulating Plan includes the figure-ground plan, general massing, build-to lines, and height limitations of buildings. The Architectural Standards describe the character of the buildings. The Landscape Plan includes the landscape and streetscape requirements. The Design Vocabulary illustrates the suggested building style, character and streetscapes.

The physical components of the plan are both written and illustrated using a form-base type code. This type of code is the most appropriate to assure the highest visual and spatial quality of town design. It assures that the redevelopment entity can expect a redevelopment of superior quality and character that will complement existing character of the town be reflective of the Vision Plan prepared for this area (attached in **Appendix 3.0**) and have a long term positive fiscal, aesthetic, and emotional impact on River Edge as the area transitions into the Town Center of River Edge. It further assures that the redeveloper can build and market a superior product that is financially feasible and finally that there is some architectural design flexibility.

GOALS OF THE PLAN

SECTION 4.0 GOALS OF THE PLAN

By adoption of this New Bridge Landing Station Redevelopment Plan, the Borough of River Edge seeks to accomplish the following goals:

- A. To eliminate underutilization of the designated Area and to eliminate blighting influences.
- B. To maximize the advantages provided by the New Bridge Landing Train Station and other mass transportation resources thereby becoming a transit oriented development.
- C. To allow for more efficient use of land and to expand the Borough's tax base by encouraging high quality mixed-use development.
- D. To build sufficient parking to meet the needs of the transit commuter, the businesses and residents of this Area.
- E. To expand the level of residential and commercial activity in the Area, thereby increasing the potential for economic activity and job creation.
- G. To maximize the participation of private developer(s) while minimizing the participation of the public sector.
- H. To create a well planned and designed development Area which will provide opportunities for mixed-use, retail, multi-family residential, entertainment, recreation, permanent employment, and commercial facilities within an area that is currently underutilized but has the potential for sound development that will improve quality-of-life and the visual and spatial character of River Edge.
- I. To provide a system of sidewalks, crosswalks streetscapes, landscapes and building wall treatments at the ground level that encourage a safe, engaging and pedestrian-friendly experience that will enhance walkability.
- M. To provide for the creation of places and pedestrian realms which promote pedestrian activities, social interaction and citizen security.
- N. To enhance the use, visual character, safety, and functionality of the New Bridge Landing Station.
- O. To provide market-value housing and commercial and retail development through new construction of mixed-use buildings.
- P. To encourage more people to take up permanent residence within transit-oriented development and to walk to transit and adjacent uses.
- Q. Design buildings for modern, mixed-uses.
- R. To create mixed use buildings, parks, plazas, streetscapes and pedestrian realms that will act as a focus area for the Borough as a transit center.
- S. To begin the first phases of developing a real "Main Street" and town center for the Borough of River Edge.
- T. To utilize Coles Brook's edge as an amenity in the Plan
- U. To create opportunities for shared parking thereby not requiring all lots to accommodate their individual parking needs.
- V. To provide high quality building design
- W. To maximize energy savings through the use of green building methods, compact design, and walkability.
- X. To maximize the adjacency to New Bridge Landing park
- Y. To preserve the existing supply of commuter parking at NJ Transit's North Hackensack Rail Station and provide for future demand.
- Z. To encourage appropriate economic development while minimizing its costs on the public resources and facilities of the Borough of River Edge.

The goals set forth are intended to be broad, general policy statements. While it is possible that specific aspects of the Plan may result in minor variations or deviations from these goals, it is intended that the overall result of the implementation of this Plan will be consistent with the goals as set forth above.

OVERVIEW OF THE PLAN

5.1 THE ILLUSTRATIVE SITE PLAN

The Area that is the subject of this Plan presents a unique opportunity for the Borough of River Edge. The site is ideally located. It is currently surface parking for the transit stop, and vacant, wooded land. This is an ideal opportunity to maximize the benefits of transit by transforming this underutilized land into a transit-oriented development. The Area has the potential for a level and quality of development that will transform the area, redefining the station area from an open parking lot to an area that will be a major contributor to a highly desirable pedestrian oriented mixed-use classic, yet modern, town center.

The plan focuses redevelopment around the NJTransit stop while promoting pedestrian connections to this important resource. The Plan focuses on building a townscape appropriate to the location while adopting the standards of Smart Growth, Transit Villages, walkability, and safety through “eyes on the street.” from residential units and active transparent store fronts with quality streetscapes.

The site is a challenge and an opportunity for innovative design. The site has a particular shape. Portions of the site lie within the 100 year floodplain. Part of the site is used for parking. One edge has a dirty stream, and it is adjacent to undistinguished tall office towers. The opportunities are its location immediately adjacent to the existing train stop; it is the first phase of the development of a new transit oriented town center for the Borough and its proximity to the remainder of the Area in Need of Redevelopment and has the ability to be phased.

The Plan meets these challenges and focuses on the creation of great pedestrian friendly streets by sitting buildings at street edges; enhancing the pedestrian realm by wide tree lined sidewalks and crosswalks; creating a new transit plaza; screening the shared parking by using liner buildings; enhancing sidewalk activity through ground floor retail; creating a very desirable place to shop, work, live and plan through building and street design; providing unique architectural facade treatment which emphasizes the 3-6 story base of the buildings..

The Plan is intended to create well proportioned and human scaled buildings and street spaces through the use of build-to-lines, setbacks and stepbacks, apparent building sized, bay spacing, roofs and cornice treatment, landmarking of corners, street proportions, and a pallet of materials and complementary landscaping.

In order to demonstrate how the Area could be structured so as to optimize its potential, a concept Illustrative Site Plan for the Area is presented in Exhibit 4. This Illustrative Plan shows the optimum configuration of the mixed-use buildings, the location and character of the pedestrian realm, the park, plazas and other openspaces, and the recommended locations of ingress and egress from the parking.

As can be seen on **Exhibit 4.0**, it is the intention of the plan that flat roofs and terraces be “green”, i.e. covered with landscaping and accessible, thereby enhancing the ecological appeal, decreasing runoff, lowering heat and energy costs and reducing urban heat island effect.

The New Bridge Landing Station Redevelopment can fulfill the role of capitalizing on underutilized land while promoting town design that provides a pedestrian friendly environment necessary for high quality mixed-use residential/commercial buildings and parking while bringing new street life to this area. When developed, the Plan will add market-rate housing, the required proportion of affordable housing, expand the range of retail, office, and possible live/work offerings, and provide a catalyst for future redevelopment within adjacent areas.

The Illustrative Plan is an example of how this Redevelopment Plan could be implemented. It is a valuable tool to visualize the location of buildings, the streetscape, the pedestrian realm and open space. Actual building form may vary as to its particulars, but the key elements of the Illustrative Plan, such as the building locations, new street network, arcades, enhanced pedestrian realms, the land devoted to open spaces and plazas and the careful massing of structures to provide both architectural interest and appropriate levels of light and air must substantially conform to this ground plan. Small deviations are expected and allowed.

5.2 PROPOSED REDEVELOPMENT ACTIONS

In carrying out the Plan, the Borough of River Edge and the designated redeveloper intends to undertake a variety of redevelopment actions. These will include but not be limited to:

- A. Clearance of all dilapidated and under-utilized structures, surfaces and other debris.

SECTION 5.0 OVERVIEW OF THE PLAN

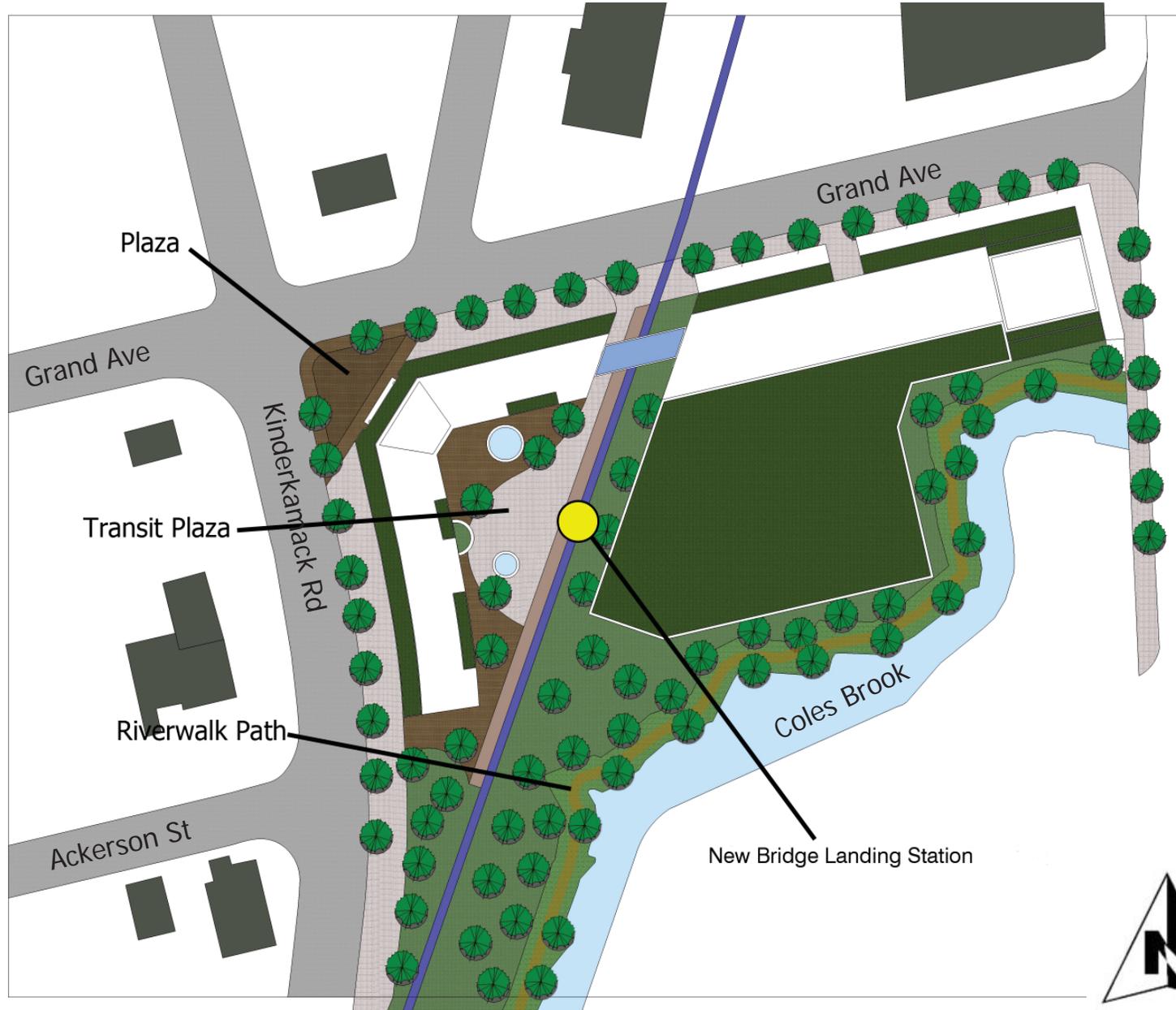
B. Assembly of suitable parcels of publically owned land for the construction of the proposed uses set forth in this Redevelopment Plan. These uses may include: retail, residential, offices, hotel, structured parking, mixed use, live-work units, plazas, parks and pedestrian walkways.

C. Improvement of streetscapes, crosswalks, pedestrian amenities and the entire pedestrian realm.

D. Construction of private buildings and other improvements appropriate to the purpose of this Plan.

E. Improvement, revitalization and beautification of the Area.

F. Construction of the “first phase” of a transit oriented, town center and Main Street for the Borough.



DEFINITIONS

SECTION 6.0 DEFINITIONS

Terms in this Redevelopment Plan shall be defined as indicated:

Area – Shall mean the New Bridge Landing Redevelopment Area

Bay – The distance between structural elements of a building that is reflected on the facade of a building.

Borough – The Borough of River Edge, NJ.

Build-to-line – A line along which the primary facades of a building must be located. The build-to-line allows flexibility to the articulation of the facade, allowing the facade to deviate in limited increments from this line.

Building Height – For the purpose of this Redevelopment Plan, the height of the buildings will be expressed in number of stories, with a range of floor to floor heights, counting up from the adjacent ground plane or sidewalk.

Context – The particular combination of elements that create specific habitat. Context includes building use, density, height and setback, and other elements of the intended habitat, including those of the private lot and building as well as those of the enfronting public streetscape.

Corridor-Urban – A linear geographic area incorporating buildings typically on both sides of the street including the pedestrian realm all in proper proportions to create a sense of street space.

Curb – The edge of the vehicular pavement detailed as a raised masonry step. The Curb usually incorporates the street drainage inlets.

Density – The number of dwelling units within a standard measure of land area, usually given as units per acre.

Developable Area – The developable area is the allowable building footprint of the ground floor of the building. It is within this designated area on the specific parcel that the building can be located.

Design Speed – The velocity at which a thoroughfare tends to be driven without the constraints of sign age or enforcement. There are three ranges of speed: Very Low (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High (above 35 MPH). Lane width is determined by desired design speed.

Elevation (drawing) – The exterior walls of a building. An elevation drawing includes material, rendered window and door openings, height and facade details. See Facade.

Encroachment – An area beyond the build-to-line that certain building element can protrude. The encroachment distance is typically expressed in feet. Typical encroachments may include overhangs, bow and bay windows, signage, porticos, steps and stoops or other elements that commonly protrude over the main facade of a building.

Façade – Any vertical, exterior face or wall of a building.

Green Roof - a roof of a building that is partially or completely covered with plants that can be used to retain stormwater, as well as for recreation (both passive and active). See **Section 10.1.7** for further detail and specifications.

Identifiable Building Widths – A subdivision of a linear facade to look like a series of adjoined buildings.

Live-Work – A dwelling unit that contains a commercial component. The commercial component can be located anywhere within the unit but is typically located on the ground floor connected internally with a stair to a residential unit. The “work” portion of the live-work unit must have a separate entrance from the “live” portion. The two portions can be adjoined by a connecting door or staircase that is internal to the live-work unit.

Mandatory Standards – Phrases or sentences that contain the following words: require, must, and shall

Manufacturing operations – Any business that engages in the making of goods or wares by manual labor or by machinery.

SECTION 6.0 DEFINITIONS

Plan – Shall mean the New Bridge Landing Redevelopment Plan.

Public Realm – Shall mean the areas that are both used and seen by a person walking.

Redevelopment Entity – Shall mean the Borough of River Edge Council or designated entity.

ROW – Right Of Way

Sidewalk – The paved layer of the public frontage dedicated exclusively to pedestrian activity.

Semi-Public space – The yard area in front of a residential unit defined by a low fence and/or gate through which a person must pass in order to gain access to the front primary entrance. Semi-public spaces are also typically located in front of smaller offices.

Setback – The required number of feet that a building must be setback from a specified line, be that a property line, right-of-way line, stepback line or curb line.

Stepback – The location where the building must be stepped back from the lower facade plane.

Street – A throughway that has emphasis on both vehicular and pedestrian movements

Streetwall – The elevations of buildings that when seen from the street or sidewalks form the space container.

Suggestive Standards – Sentences or phrases that include the following words: recommended, may, suggests

Yard, Front – An open and unoccupied (except for driveways) space, unless occupied by a use as hereinafter specifically allowed, extending across the full width of the lot and lying between the front street property line and the

nearest line of the building.

Yard, Side – An open and unoccupied space, unless occupied by a use as hereinafter specifically allowed, on the same lot with the building between the building and the side lot line, extending from the front yard to the rear yard.

Yard, Rear – A space unoccupied except by an ancillary building structure or use as hereinafter specifically allowed, extending across the full width of the lot between the rear line of any building, other than an ancillary building, and the rear lot line.

MOBILITY PLAN

7.1 THE CONCEPT DESIGN

Streets are the Borough’s most important public spaces. Streets function as the circulation for both vehicles and pedestrians. Streets must not only be functional but also beautiful and safe. It is from these streets that the perceived character of the town center will be determined. Streets include not only the cartway, curb and parking edge, but also the landscaping, streetscaping, sidewalks, arcades, signing and building or street wall. Streets play a key factor in market “curb” appeal.

The functional, aesthetic and perceptual characteristics of streets must be positively optimized and understood as a component of townscape, resulting in places with the highest value and quality of life. It is the design of the streets, streetwall, street network, vistas and landmarks that allow easy, legible and understandable movement of both vehicles and pedestrians throughout the site and the town.

The Mobility Plan includes a Parking Plan and a Pedestrian Circulation Plan.

7.2 PEDESTRIAN CIRCULATION PLAN

The signs of a successful townscape are large numbers of people walking on the sidewalks. People will walk if there are pleasant and engaging places to walk and destinations on both ends of the walk.

The Pedestrian Circulation Plan must be designed to encourage walkability along the street edges, and across streets, with safe pedestrian, crossing of the rail at grade as well as via skyway connecting the parking to the train station. Walking must be safe a pleasant to and from - retail shops and offices, the train station, institutional and civic places, adjacent neighborhoods, and along the proposed Riverwalk. This is a transit-oriented development, putting an even greater reliance on people walking.

The Pedestrian Circulation Plan is a key component of Transit-Oriented Development by providing a safe and pleasant walking experience to and from the NJT rail station. Pedestrianism is dependent on the adequacy of sidewalk width, quality, safety and visual interest of the walking experience. There must be a continuity of the pedestrian experience including crosswalks that maintain sidewalk textures/treatments across the streets while providing appropriate lighting, and the landscape treatment of the entire pedestrian realm. Because the peak pedestrian travel times are expected to be morning and evening work trips, during lunch, on weekend days and during evening dining hours, lighting and retail availability along all sidewalks and the River walk must be coordinated.

The Pedestrian Circulation Plan, illustrated in **Exhibit 5.0**, is a continuous network of sidewalks, paths, crosswalks and plazas designed to provide a positive walking experience that focuses on the train station.

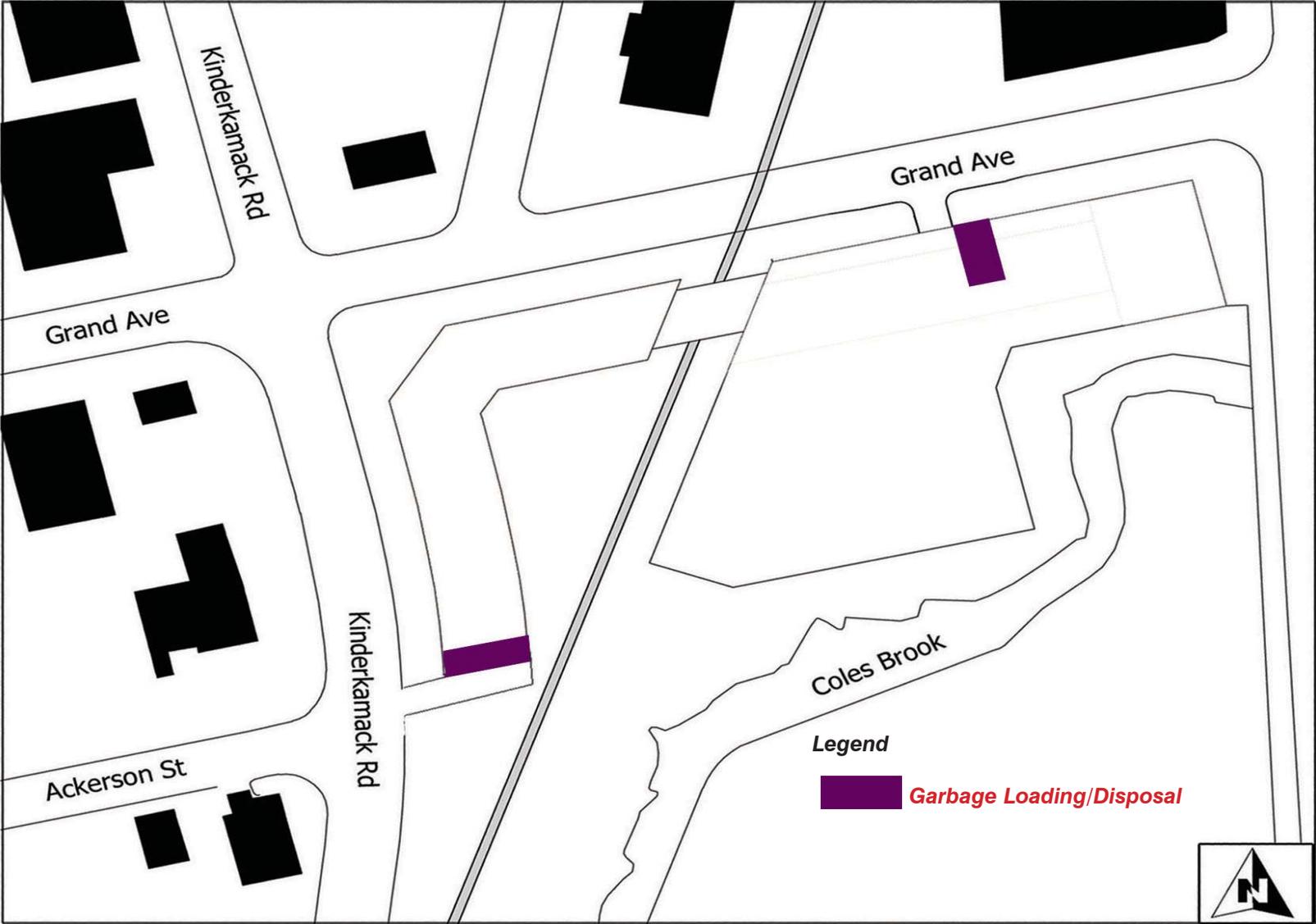
The required pedestrian circulation elements are as follows:

- A. Sidewalks shall be continuous along each block face that has street frontage.
- B. Sidewalks must be a minimum of 15 feet wide containing sufficient area for tree planting and decorative lighting and other street furniture like trash baskets, bicycle racks and benches. In specified locations and providing they meet specified standards, outdoor cafes are allowed.
- C. Proposed lighting standards and sidewalk materials shall match those currently installed on Kinderkamack Road.
- D. Crosswalks: Pedestrian crosswalks are required at locations specified in the Pedestrian Circulation Plan. This should be a very wide crosswalk, minimum of 15 feet and if possible raised 4 to 6 inches above the grade of the street to promote pedestrian safety. The interior of the intersection formed by the crosswalks shall be textured with cobblestones or other rough paving material that will encourage traffic calming. Pedestrian activated traffic light is highly recommended.
- E. Skyway over railroad connecting parking to the station will be a minimum of 25 feet above top of rail (per NJ Transit standards).

7.3 LOADING AND DISPOSAL REQUIREMENTS

- A. Each building shall be designed so as to accommodate easy access to garbage dumpsters and recyclable material containers by residents, tenants and carters. Enclosed garbage and recycling facilities shall be provided within each building and be sized for expected users. Recommended locations are shown on **Exhibit 6.0**.
- B. All garbage and recycling shall be screened from view from the exterior of the building.





C. Goods and services: package delivery for ground level retail and office can be across the sidewalk provided that a loading zone is designated on the curb edge.

7.4 PARKING REQUIREMENTS

Use	Parking Requirement	
	minimum	maximum
Residential	1.25 spaces/unit	1.5 spaces/unit
Retail	3.0 spaces per 1,000 sf	4.0 spaces per 1,000 sf
Office	3.0 spaces per 1,000 sf	4.0 spaces per 1,000 sf
Hotel	0.75 spaces per room	1.25 spaces per room
Restaurant	1.0 space per 4 seated table spaces	2.0 spaces per 4 seated table spaces

The minimum and maximum number of spaces for commercial (retail or office) and residential is provided in the chart above. Parking shall be provided at a minimum number of spaces and cannot exceed the maximum. The parking spaces per unit for any provided residential unit in any of the proposed buildings, is specified regardless of number of bedrooms. Parking for any unit or commercial uses that desire additional parking spaces shall be provided in ancillary parking or shall take advantage of any shared parking opportunities that will occur in the parking facility. This has an additional advantage of further activating the streetscape, improving street safety, and freeing land to be available for a transit station plaza. The number of spaces allocated for commercial uses shall be based on the gross square footage of the commercial space. Any on-street parking parallel on the redeveloped side of the street in the Area can count to meet the minimum need for commercial uses.

The maximum number of parking spaces has been specified because this redevelopment is transit-oriented, with walkable destinations and transportation, and is expected to attract a high percentage of empty nester older households that statistically have fewer cars.

Shared Parking: In order to promote more efficient use of parking facilities, a parking space may be counted towards the parking requirement for two or more different uses, provided that:

1. The applicant exhibits to the Planning Board’s satisfaction that demand for these shared parking spaces by each use, based on time of day, will not significantly overlap; and
2. No more than 50 percent of the parking spaces counted toward any use are shared spaces.

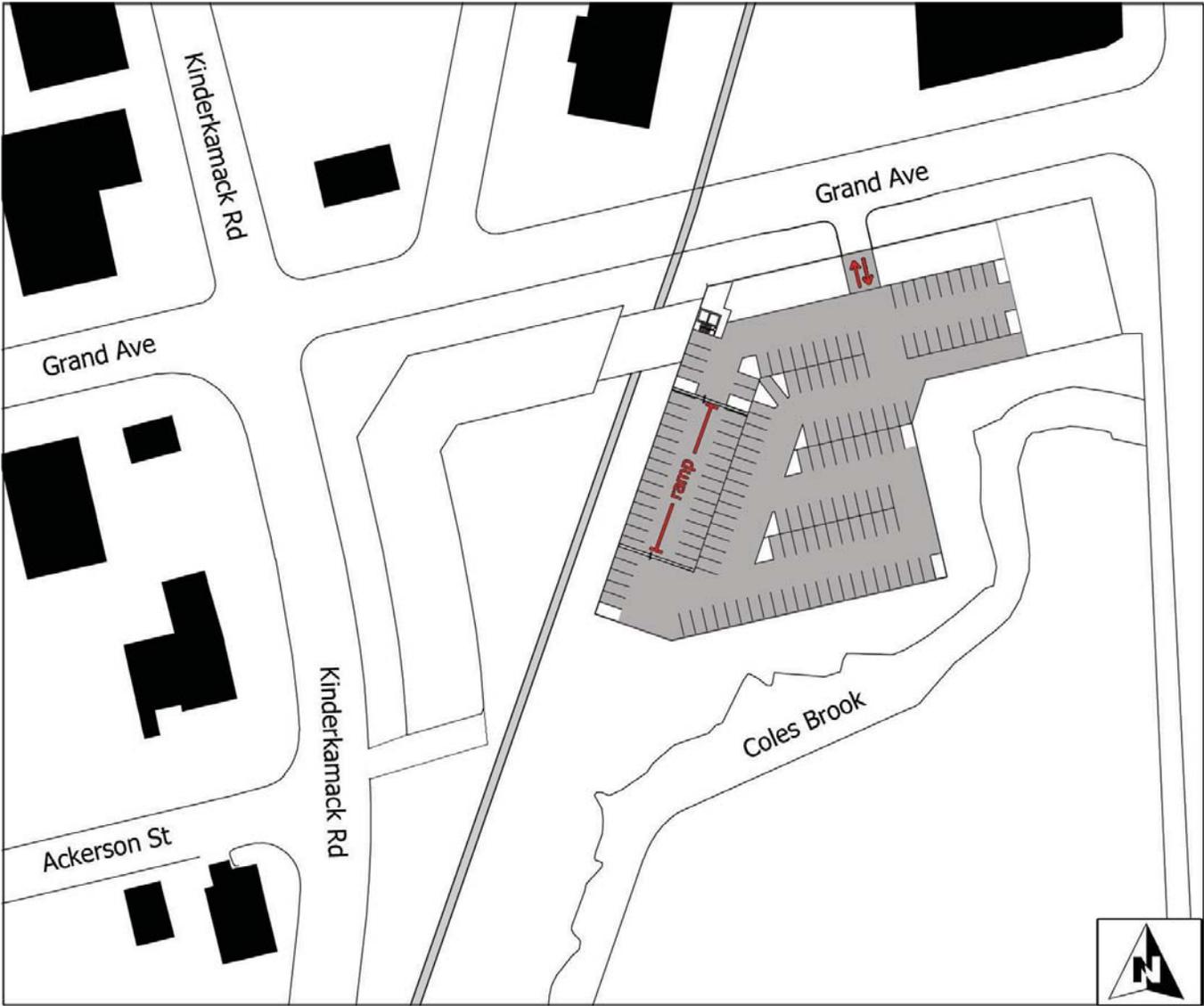
A potential developer must perform a traffic study and parking plan which must be approved by the Planning Board before development begins.

7.5 ADDITIONAL PARKING REQUIREMENTS

A minimum of 500 to a maximum of 550 parking spaces shall be provided for New Jersey Transit for use by commuters.

7.6 PARKING PLAN

Exhibit 7.0 illustrates a potential configuration of the parking structure on Redevelopment Parcel B. The travel lanes of the entire structure are two-way. This configuration supplies approximately 150 spaces per level. See **Exhibit 14.2 and 14.3** for a cross-section and the building regulations for this structure.



LAND USE PLAN

SECTION 8.0 LAND USE PLAN

8.1 GENERAL REQUIREMENTS

In the event of any conflict or inconsistency between the provisions of this Plan and the provisions of The Borough of River Edge's Land Use Regulations, this Plan governs. The continued use of existing properties made non-conforming by adoption of this Plan is permitted until the property is to be redeveloped or substantially rehabilitated, at which time the provisions of this Plan shall apply. In the case where a particular land use or site standard is not covered in this Plan, compliance with the Borough of River Edge's Land Use Regulations or other applicable codes or Ordinances will be required.

Specific application of the land use and development requirements of this Plan, as they affect existing uses will be as follows:

- A. Existing uses that are non-conforming with the current zoning provisions will remain non-conforming unless they are expressly permitted in this Redevelopment Plan.
- B. Existing principal or accessory uses of properties, permitted by the use provisions of the Land Use Regulations in effect for the Area immediately prior to the effective date of this Plan, but which are not listed as permitted uses in this Plan, will become prior non-conforming uses at the time this Plan is effective. At that time and thereafter, any modification or expansion of these prior non-conforming uses is prohibited and any new use, redevelopment or rehabilitation of such properties shall be subject to the provisions of this Plan.

8.1.1 Exceptions to the General Requirements

Variation(s) from the development requirements and design standards set forth by the Plan may be necessary in certain limited circumstances. In such circumstances, the Planning Board may grant reasonable exceptions from certain bulk or design requirements if the designated redeveloper demonstrates that such design exceptions will not substantially impair the intent of the redevelopment plan, and will not present a substantial detriment to the public health, safety, and welfare.

To gain approval of such modification or waiver of a development requirement or design standard, the applicant shall demonstrate that the resulting change will:

- A. Generally satisfy the Redevelopment Plan's goals and purposes;
- B. Be designed in accordance with the Borough of River Edge's normal acceptable engineering, planning and/or architectural practices;
- C. Not have any adverse impact on the physical, visual, or spatial characteristics of the overall development plan for the parcel or tract to be developed, or adjacent or nearby properties;
- D. Generally enhance the overall development plan for the tract;
- E. Not have an adverse impact on the physical, visual or spatial characteristics of the existing streetscape in which such development is located or of the Redevelopment Plan;
- F. Not reduce the useful life or increase the costs of maintenance of the improvement to be modified or otherwise have an adverse impact on the long-term function of the development; and

Deviations from the uses permitted in the Area shall be permitted only by means of an amendment of the Redevelopment Plan by the Borough of River Edge, should the deviation be considered acceptable to the furtherance of the goals and purposes of this Plan.

8.1.2 Provisions Related to Rehabilitation

Rehabilitation and redevelopment of all buildings and improvements in the redevelopment area shall be made by property owners and/or developers in accordance with the standards included in this Plan as well as all other applicable Borough of River Edge and State codes, regulations and standards.

8.1.3 Provisions Related to Off-Site Improvements

The extent of the Redeveloper's responsibility for any installation or upgrade of infrastructure related to their project, whether on-site or off-site, will be outlined in the Redeveloper's Agreement with the Borough of River Edge. Off-site responsibility for properties not covered under the redeveloper's agreement will be determined in the same manner as other development projects throughout the Borough of River Edge during the permit and/or site plan review phases.

All infrastructure improvements shall comply with applicable local state and federal codes including the Americans with Disabilities Act. All streetscape improvements shall also comply with applicable standards found in this Plan. All utilities shall be placed underground, unless otherwise authorized by the Borough of River Edge.

8.1.4 Supplementary Submission Requirements

The planned development shall meet the following submission requirements, which shall supplement submission requirements otherwise specified for applications to the Borough of River Edge Planning Board:

- A. A general land use plan at a scale specified by ordinance indicating the tract area and general locations of the land uses to be included in the planned development.
- B. A circulation plan showing the general locations and types of transportation facilities, including facilities for pedestrian access, within the planned development and any proposed improvements to the existing transportation system outside the planned development;
- C. A utility plan indicating the need for and showing the proposed location of sewage and water lines, any drainage facilities necessitated by the physical characteristics of the site, proposed methods for handling solid waste disposal, and a plan for the operation and maintenance of proposed utilities;
- D. A storm water management plan setting forth the proposed method of controlling and managing storm water on the site;
- E. An environmental inventory including a general description of the vegetation, soils, topography, geology, surface hydrology, climate and cultural resources of the site, existing man-made structures or features and the probable impact of the development on the environmental attributes of the site;
- F. A local service plan indicating those public services which the applicant proposes to provide and which may include, but not be limited to, water, sewer, cable and solid waste disposal;

G. A fiscal report describing the anticipated demand on municipal services to be generated by the planned development and any other financial impacts to be faced by the Borough of River Edge as a result of the completion of the planned development;

H. A housing plan outlining the number of housing units, if any, to be provided and the extent to which any housing obligation assigned to the Borough of River Edge will be fulfilled by the development; and

I. A proposed timing schedule in the case of a planned development.

Affordable Housing Component: In no event shall a redevelopment project constructed pursuant to this Plan result in an additional obligation to the Borough of River Edge to provide affordable housing pursuant to policy or regulations of the Council on Affordable Housing unless such additional obligation is met by the developer of such project pursuant to a redeveloper agreement between the developer of such project pursuant to a redeveloper agreement. All costs to the Borough of River Edge, including professional fees associated with assisting the Borough of River Edge in addressing the impact of a redevelopment project under this Plan on the Borough's affordable housing obligation, shall be paid by the developer.

Any deviations from the use requirements or affordable housing provisions of this Redevelopment Plan shall be addressed as an amendment to the Redevelopment Plan rather than via variance relief through the Planning Board of the Borough of River Edge.

8.1.5 Procedure for Amending the Plan

This Plan may be amended from time to time in accordance with the procedures of the LRHL. An appropriate fee, as determined by the Borough, shall be paid by the party requesting such amendment, unless the request is issued from an agency of the Borough. The Borough, at its sole discretion, may require the party requesting the amendments to prepare a study of the impact of such amendments, which study must be prepared by a professional planner licensed in the State of New Jersey.

8.2 PERMITTED USES

The following uses shall be permitted within the Redevelopment Area.

- A. Offices: General and professional offices, including, by way of example, but not limitation, offices for doctors', dentists', lawyers' accountants' and architects' offices provided they do not occupy the ground floor. The only exception is if there is a market limitation for retail uses on the provided front street edge. If offices are provided, the front edge (semi-public space) along the street must be landscaped.
- B. Retail: Stores, restaurants, and similar facilities including by way of example, but not limitation markets, stores, convenience stores, entertainment establishments, restaurants, cafes, bars, health and fitness clubs, clothing stores, banks and boutique retailers. Outdoor dining is permitted at specified locations and further subject to clear pedestrian passage as specified later in the Design Standards.
- C. Parking: Premises built within a structure for uses in the redevelopment area and used exclusively for the parking of automobiles and other vehicles primarily to support those uses. Parking is also permitted, and expected, to be supplied for NJTransit commuters. Parking structures are subject to review for compliance to Design Standards and NJTransit 2005 Guildlines and Standards Manual.
- D. Multi-Family Residential: Includes premises available for long-term human habitation by means of ownership or rental, but excluding premises offered for rental for periods of less than a month's duration; excludes all boarding houses and rooming houses.
- E. Hotels: Small hotels with a minimum of 75 rooms and with attached structured parking and access to a park, or riverwalk.
- F. Live work units: Those units that contain a residential portion of the unit attached or directly accessible to the work portion. There can be a limited number of employees, based on size, outside those that directly inhabit in the residential portion of the unit. The work portion cannot exceed 50% of the total space.
- G. Mixed-uses that combine any two or more of the permitted uses

within a single building. The ground floor retail locations are shown in the **Exhibit 8.1**. Along these edges retail and services that have walk-in customers are allowed and encouraged. Offices are allowed on the upper levels with lobby access on the lower levels. If provided at the lower level along the sidewalk edge, a semipublic space of no less than 4 feet shall be required that is extensively landscaped thereby obstructing views from passersby into offices. An appropriate sized access sidewalk shall be provided from the sidewalk to the entrance of these lowered floor offices.

8.3 INTERIM USES

The following shall be permitted interim uses in the Area. The duration during which such uses may remain in place shall be determined by the Redevelopment Entity and shall be reflected within the Redeveloper Agreements as may be executed between the Redevelopment Entity and the designated Redeveloper.

- A. Surface parking facilities
- B. Any other use approved within the Redeveloper Agreement that will not encumber the property in any way as would hinder the ultimate development of the permanent permitted use
- C. Construction Staging Area

8.4 PROHIBITED USES

Any use not expressly permitted as either a permitted use or as an interim use shall be prohibited within the Redevelopment Area. The following list includes examples of uses that are prohibited and is not intended to be exhaustive of all prohibited uses.

- A. Drive-through commercial uses where patrons remain in automobiles,
- B. Gas and service stations and car wash facilities.
- C. Vending machines, except when located within buildings.
- D. Commercial kennels and animal husbandry facilities.
- E. Warehousing, distribution or storage facilities.

SECTION 8.0 LAND USE PLAN

- F. Automotive sales, repair, cleaning or long-term storage or on-grade parking lots.
- G. Junk yards and recycling facilities.
- H. Labor pool buildings, halfway houses, and food pantries.
- I. Industrial and/or manufacturing operations.
- J. Facilities producing adverse impacts on adjacent properties in general, usually as a result of odor, vibration, noise or pollution: excluding NJ Transit rail or bus operations in the redevelopment area.
- K. Structured parking facilities, except those required to meet the permitted parking requirements of permitted uses within the redevelopment area.
- L. Tattoo studios and adult book and novelty shops.
- M. Pool halls and night clubs.
- N. Church, synagogue, house of worship, and similar religious facility.

8.5 LAND USE PLAN

Exhibit 8 illustrates the land use regulation of the Area.

The Land Use Plan within the Redevelopment Area is flexible; however the programmed uses are specific to their respective locations. Within the three buildings, there may be approximately 70,000 square feet of office/flex space, 35,000 square feet of retail/commercial space, approximately 130 housing units based on an average unit size of 1,250 square feet (including circulation and mechanical elements) and approximately 1050 parking spaces. The program is recommended to optimize the amount of activity necessary to activate the plaza, pedestrian realm and adjacent areas. The program is approximate and will be market driven. For the plan to be successful and implementable it is important to have a diverse range of usages.

Use	Amount
Retail	approx. 35,000 sq ft
Office and/or Live-Work	approx. 70,000 sq ft
Residential	approx. 130 units
Parking	approx. 1050 spaces

The following chart denotes the minimum size of residential units by unit type. Furthermore, the practice of residents' buying multiple adjacent units for the purpose of combining them as one larger unit is encouraged.

Unit Type	Minimum Size (per unit)
Studio	500 sq ft
1 Bedroom	750 sq ft
2 Bedroom	900 sq ft

Live-Work units shall be created as the market demands. When built, these units must first occupy the the lower floors of Building #2, as noted in **Exhibit 8.3**. However, if there is a desire for additional Live-Work units, they may be located in any of the buildings, given that these units do not share circulatory elements (hallways, elevators, stairs, entrances, etc.) with purely residential units.

To maintain the desired character of the Area, retail must be located at the ground floor at the locations specified in **Exhibit 8.1**.

Because each building will contain several uses, it is important to recognize the delineation of entries, for the residential, retail and office uses. Entries shall be separate from one another, and where necessary, security front desks be implemented. Prominent (primary) entries to the residential units shall feature a marquee extending out onto the sidewalk. See Exhibit 17 to see how the building edge interacts with the pedestrian realm and street.

None of the newly constructed dwelling units in the redevelopment project shall consist of dwelling units containing three (3) or more bedrooms.

Exhibit 8.1 Land Use Plan – Retail
(ground floor)

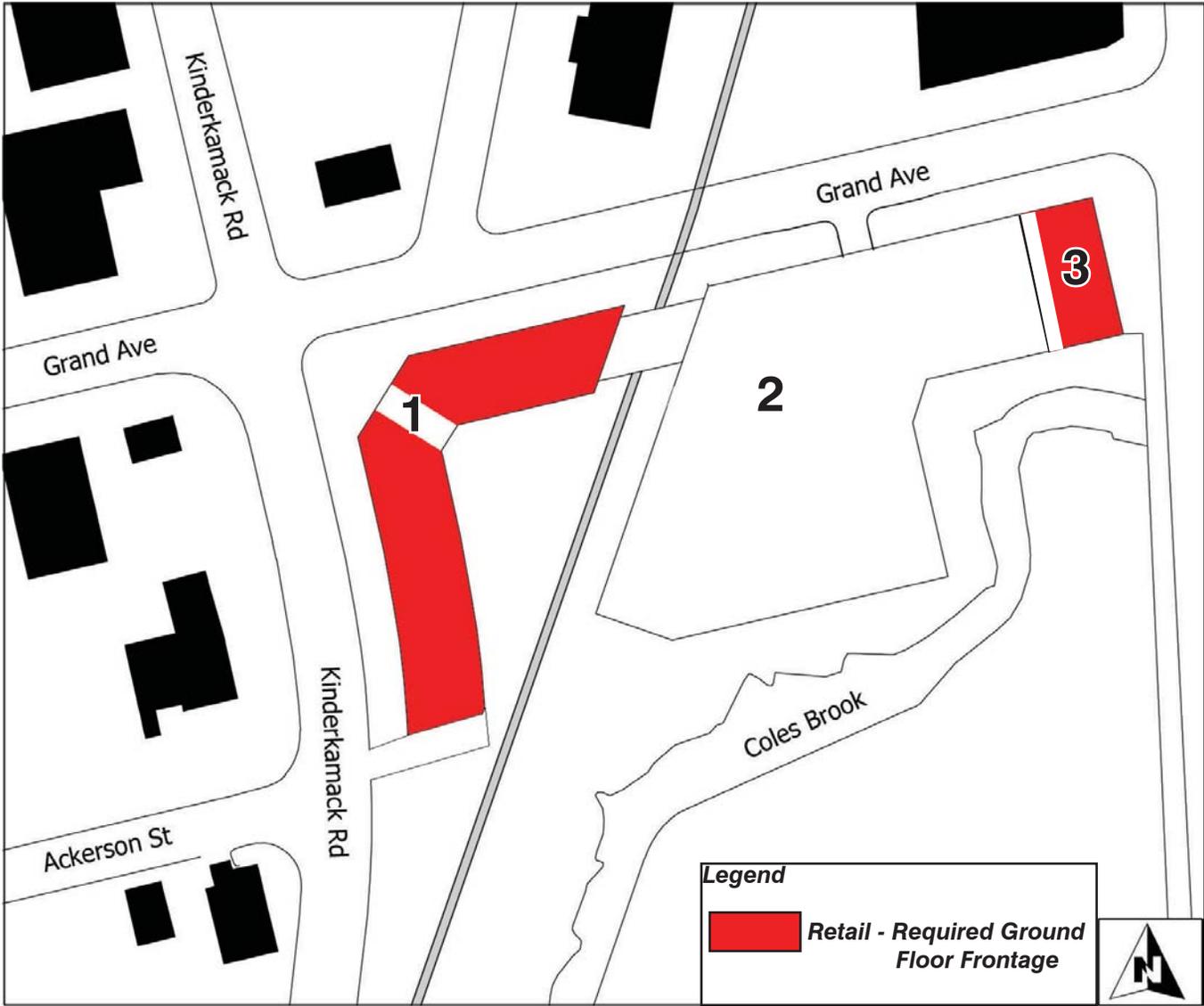
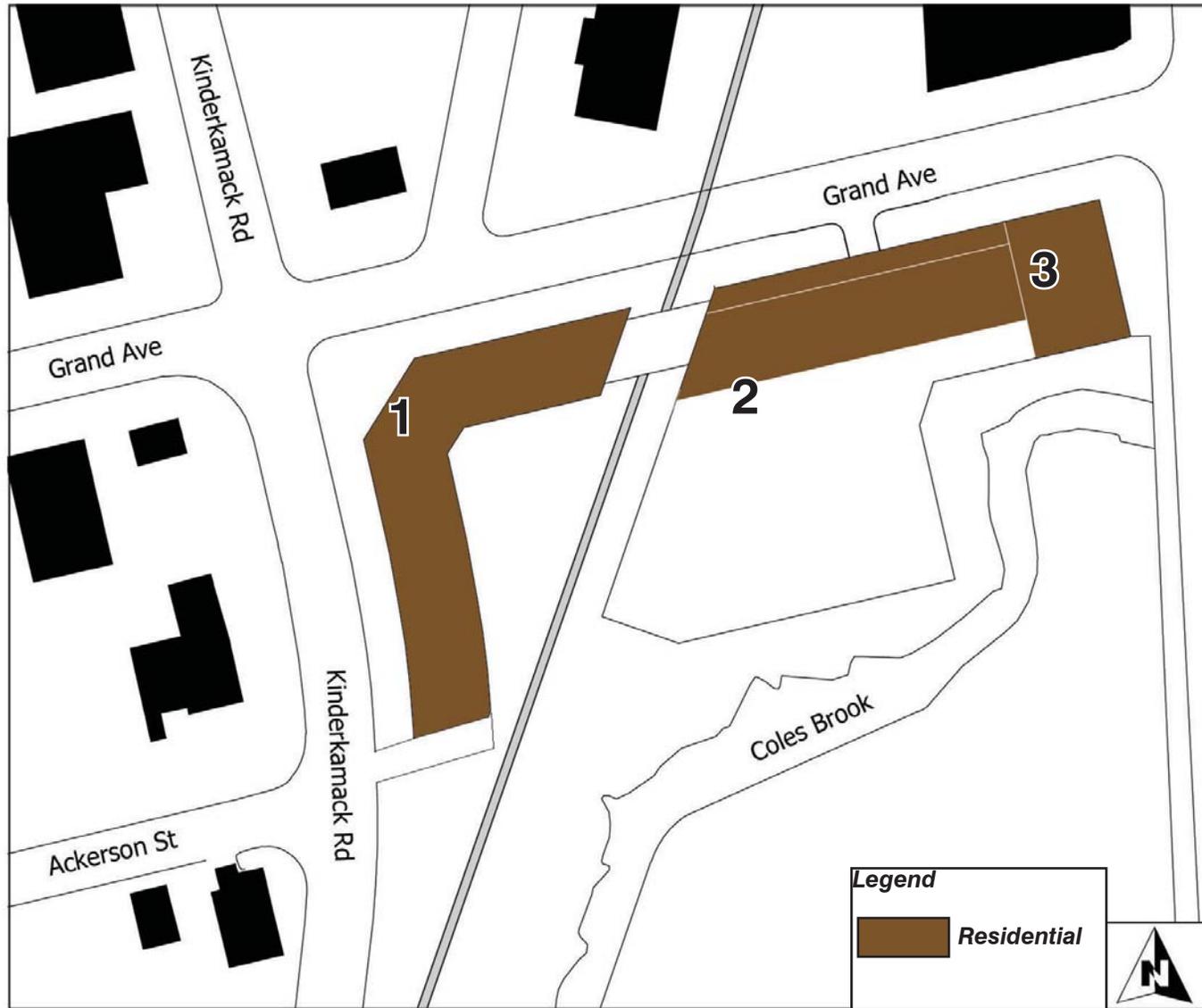
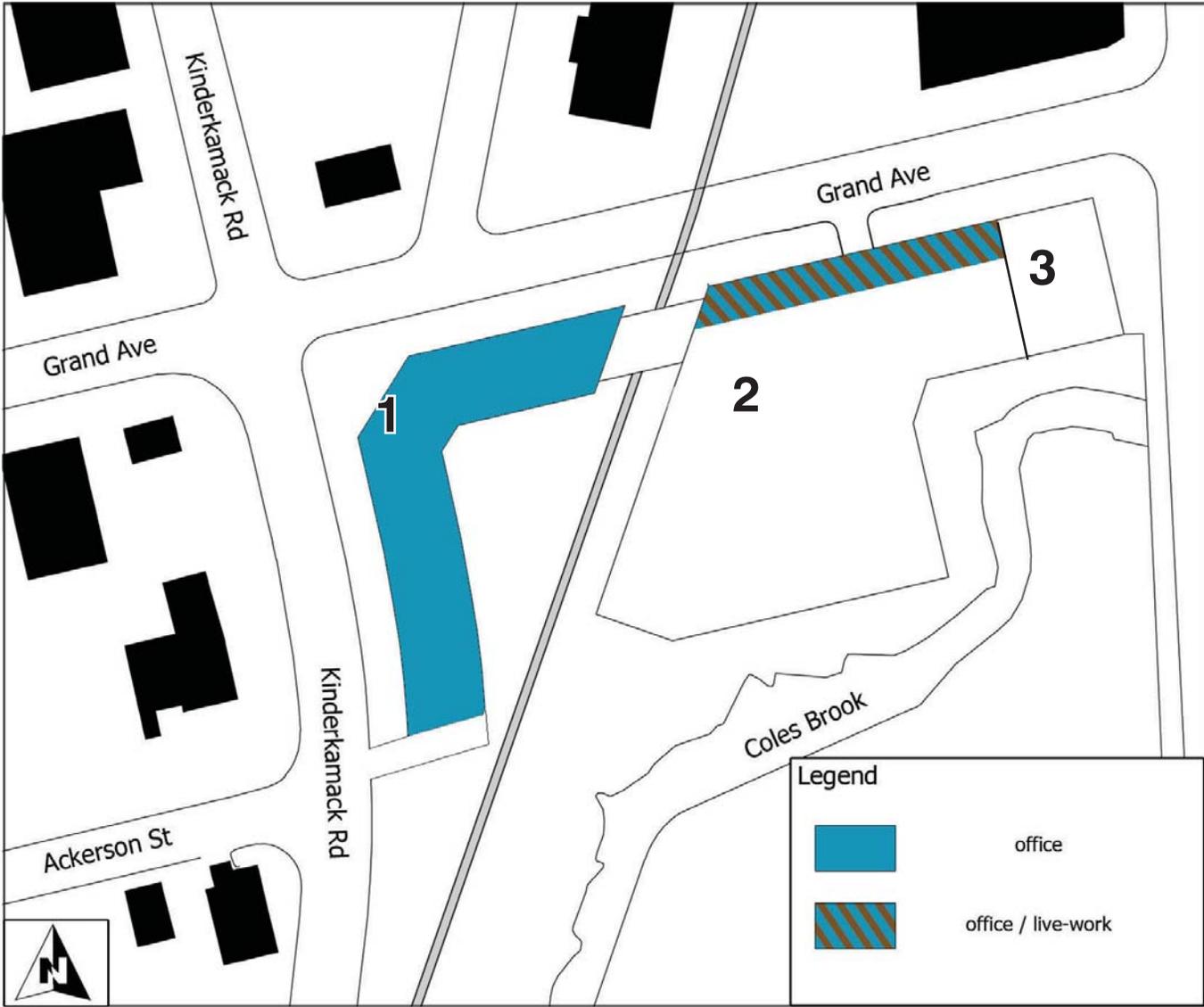
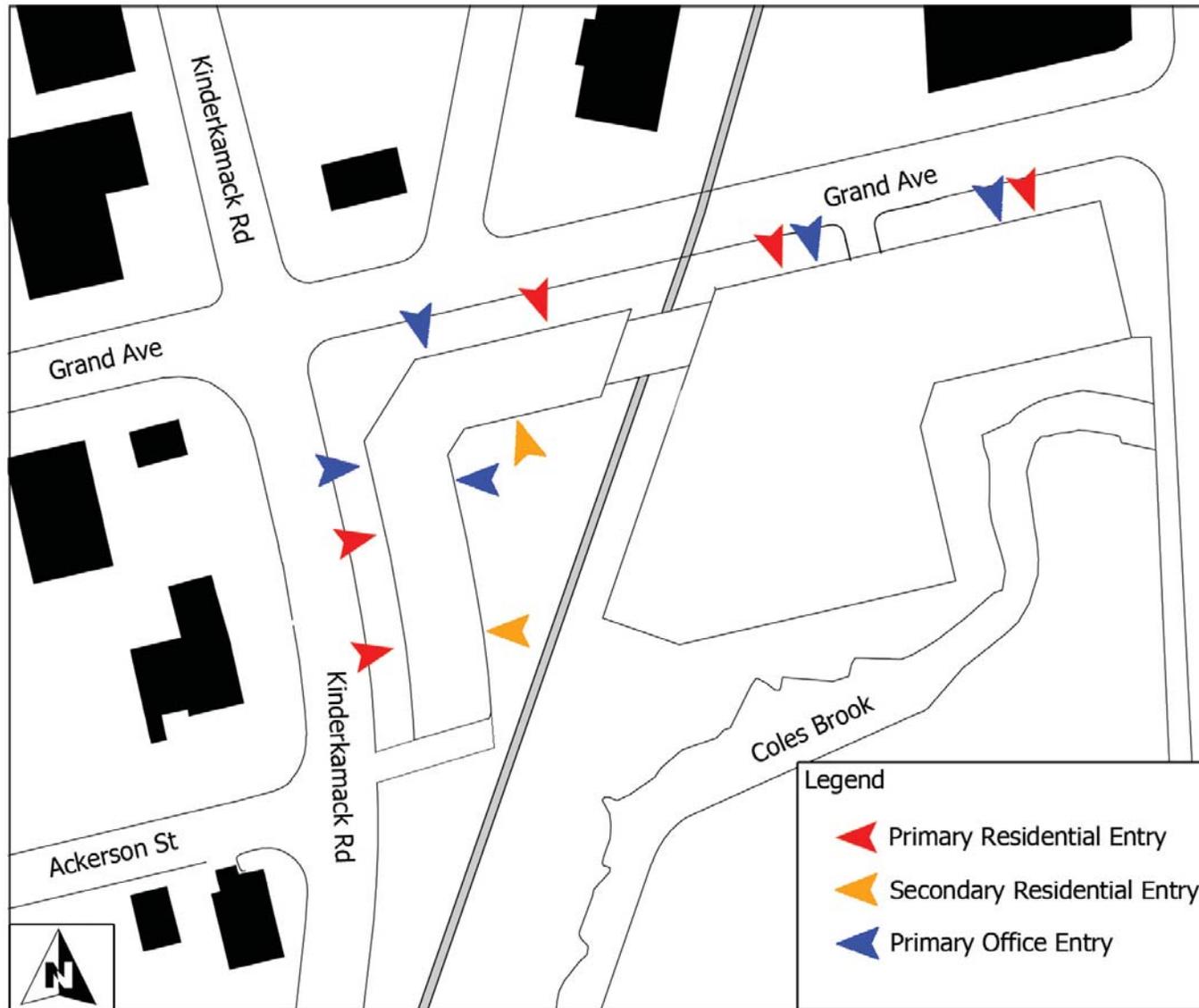


Exhibit 8.2 Land Use Plan – Residential
(above ground floor)







BUILDING REGULATIONS

9.0 BUILDING REQUIREMENTS

9.1 DEVELOPABLE PARCELS

The Area has been divided into two (2) development parcels. Up to three (3) separate buildings are allowed on these parcels. The location of each parcel is illustrated in Exhibit 3. Each parcel has been color coded. The three buildings have been identified in the Figure Ground Plan, **Exhibit 12.0**.

The development regulations for each parcel are based on a form-based regulation that determine the character of the spaces, defined by the streets, pedestrian realm along with the mass, form, detailing and materiality of the buildings.

There are two sets of fundamental regulations within this document that control the visual and spatial character: the Building Regulating Plan and the Architectural and Landscape Standards. These regulations have sufficient flexibility for the architects to design great buildings and streetscapes while insuring the minimum level of appropriate urban design standards. Included in the basic standards for a form base code are the buildable footprint, build-to-lines, encroachments, setbacks and step backs, identifiable building widths, bay spacing, bulk and height reflected through building sections and applied to the recommended street network and minimum requirements of the Transportation and Pedestrian Circulation Plans.

9.2 ENVIRONMENTAL CONDITIONS

9.2.1 Building in the 100 year floodplain

A design challenge presented by this site is its relation to the 100 year floodplain. **Exhibit 10.0** illustrates the topography and its relationship to the floodplain. The original map was generated by Christopher P. Statile, P.E. Professional Engineers & Planners, in association with Mega Engineering, Inc. and Amy S. Greene Environmental Consultants for NJTransit in January 2006.

On this map, the 100 year floodplain is at the 9 foot elevation. Topographic lines in red indicate areas of the site in the floodplain while green lines indicate areas out of the floodplain. In the 100 year floodplain, no more than 20% of the site may be filled, and use restrictions are complicated. As a design solution the bottom of the first structural floor is at the 10 foot elevation, consequently leaving the first finished floor at the 11 foot elevation. Additionally, a dual-level sidewalk is recommended as a means to carry the retail-pedestrian experience above the floodplain elevation.

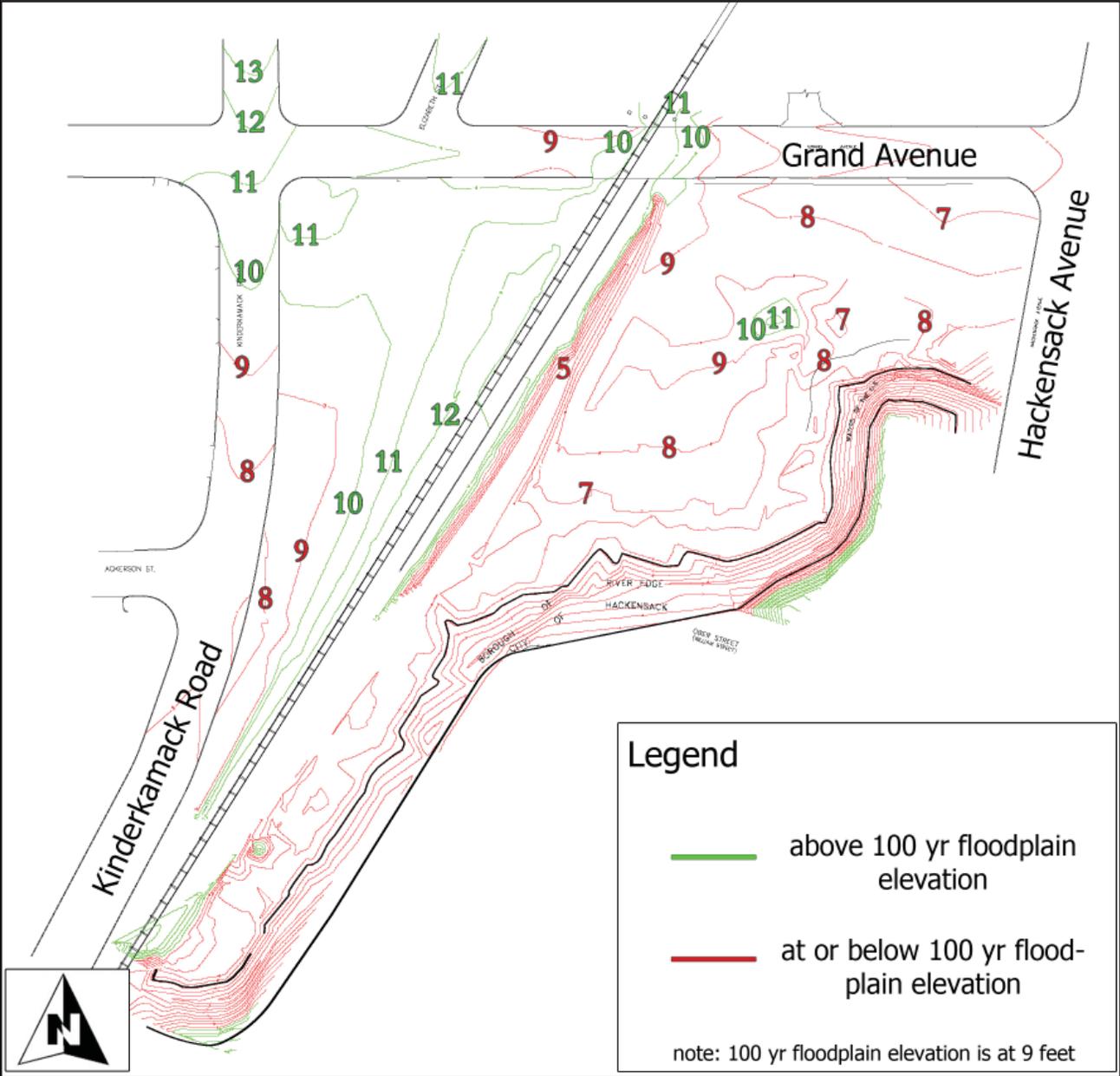
By building this way, the only fill in the floodplain would consist of the footings, beams and piles. Additional fill would result from the steps of the dual-level sidewalk. This technique minimizes disturbance of the floodplain, allowing flooding waters to flow underneath the building as needed. See **Exhibits 14.1-14.4** for cross-sectional diagrams illustrating this strategy.

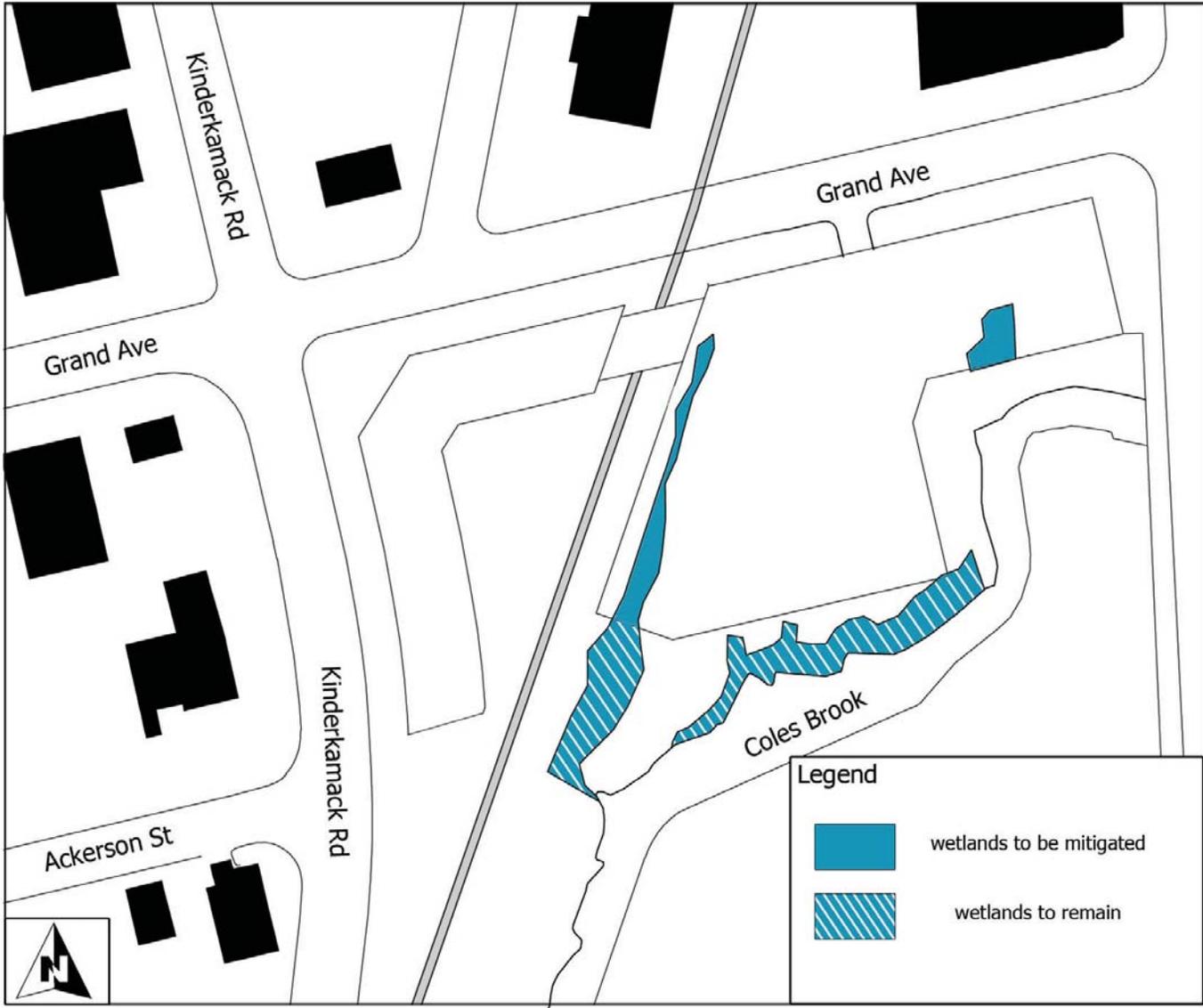
9.2.2 Wetlands Mitigation

In addition to floodplain issues, the Area also holds three wetlands areas, illustrated in **Exhibit 11.0**. In total, there is 14,560 square feet of wetlands. Due to the proposed redevelopment, 3,736 square feet of wetlands must be mitigated. Considering the amount of open space left available on the site, these 3,736 square feet may likely be mitigated within the Area.

9.2.3 Stormwater Management

Stormwater run-off will be captured, as much as possible, by the green-roofed parking structure and rainwater-collecting cisterns on other roofs. The rainwater should be reused within the Area, to irrigate the plaza and other site landscaping. If the green roof technique is not used, other means of stormwater management must be developed and approved by the Planning Board.





9.3 BUILDING REGULATIONS

The following summarizes the regulations for the buildings. Each development parcel has a maximum developable area or building footprint, a build-to-line, setbacks and stepbacks, and height and bulk standards specified.

9.3.1 Figure Ground Plan / Developable Area

The figure ground plan / developable area regulation is **Exhibit 12.0**. This illustrates the allowable building footprint of the ground floor of the building. It is within this designated area, on each parcel, that the building can be located.

9.3.2 Build-to-line

The build-to-line is the line (typically parallel to the property line) along which the primary façade of the building must be located. Build-to-lines are determined to ensure that building placement clearly defines street edges and corners. The build-to-line allows flexibility to the articulation of the facade allowing portions of the facade to deviate in limited increments from this line unless specified otherwise. Deviations using recessions along the facades are recommended to create identifiable building widths. The Build-to-lines are illustrated in **Exhibit 13.0**, which locates the build-to-lines for all parcels in the Redevelopment Area.

Along Kinderkamack Road, and Grand Street, to promote the identifiable building widths and to express the facade as a series of connected smaller buildings there can be deviations from the build-to-line of up to six feet back of the build-to line and two feet in front of the build-to-line at the locations of the various identifiable building widths. On the other facades, deviations are limited to four feet either in front or in back of the build-to-line. The build-to-line regulations assure that an appropriate street-wall and building articulation is formed. In this plan, the build-to-line is illustrated for each development parcel.

The primary lower facade before the stepbacks must respect the build-to- line. At street corners, building facades must occupy 100% of the build-to-line within 50 feet of the property corner.

9.3.3 Stepbacks and Setbacks

Stepbacks are a required design feature of all facades. See **Exhibits 14.1-14.4**. A stepback is where the building must be stepped back from a lower facade of a building along the block at a prescribed range of heights. This is required at the fourth or fifth story to insure that the scale of the buildings and the proper proportions

of the street space are respected. The three to four story stepback insures that the “small town” pedestrian scale is respected. The stepback, along with the identifiable building width, is required to create the more intimate sense of scale and a small town feeling at the lower levels, allowing a building to increase in height in back of this line, minimizing the visual impact while assuring the development’s financial feasibility. Step backs must occur at the recommended range of heights, along with the identifiable building widths and bay spacing. Stepbacks must meet the minimum stepback requirements and must be terminated by a predominant cornice.

Buildings within the Redevelopment Area must employ the stepback thereby reflecting, particularly from the ground view, the continuity of the traditional pedestrian scaled lower building facades. With the stepback, and using the identifiable building widths and the traditional bay spacing, the scale and rhythm of traditional buildings are respected which are comfortable and considered positive by pedestrians. Above the stepback, upper floors are less visible and apparent when the step back is applied.

Stepbacks also reduce the mass of the new buildings and open more of the street to sunlight. Additionally, stepbacks provide flat areas that can be used as terraces creating another amenity for the residents while generating a building that is more ecologically responsive.

The stepback is required in specified locations and is illustrated on the building sections. The stepbacks for each of the buildings are illustrated for each parcel. The stepback regulations are drawn to the maximum number of stories of the building. If a building has a lower number of stories, the lower portion of the section apply. On landmark corners the stepback is optional.

A setback is the minimum required number of feet that a building must set back from a specified line, be that a property line, right-of-way line, curb line, build-to-line or a step back line. Setbacks are specified in **Exhibits 14.1-14.4**.

The only exception to the stepback requirement is Building 2. Due to the large amount of parking required within the structure and the desire to have the parking garage hidden from street view, Building 2 may have its first stepback at the seventh floor.

SECTION 9.0 BUILDING REGULATIONS

9.3.4 Height and Mass

Height is expressed in the number of stories independent of the range of floor to floor dimensions. The tallest building in the plan, Building 3, has a maximum number of floors is twelve (12). All other building or portions of buildings heights are illustrated in **Exhibit 14**.

Floor height can and should vary. This is also illustrated in **Exhibits 14.1-14.4**. Typically the ground floors are higher (taller), the middle floors are lower and the top floors are again higher. The building sections indicate those floors that can have additional height. This is encouraged to add interest and greater marketability to the building. The height is limited to a number of stories above the adjacent ground/sidewalk grade. The following chart regulates the floor heights by regulating the floor-to-ceiling height. The floor-to-ceiling height is the vertical distance from the finished floor to the ceiling. In instances of architectural expression at the roof line, such as a curved roof (shown in **Exhibits 14.1-14.4**), or in instances where residents buy vertically adjacent multiple units for the purpose of creating one larger unit, the regulations given in this chart may be amended pursuant to Planning Board approval.

Regulations for Floor-to-Ceiling heights per use and building:

Floor Type	Building #1	Building #2	Building #3
Ground Floor Retail	9 feet to 15 feet	n/a	9 feet to 15 feet
Ground Floor Office	n/a	9 feet to 12 feet	n/a
Non-Ground Floor Office	9 feet to 12 feet	9 feet to 12 feet	n/a
Upper Residential Floors (top 3 floors)*	9 feet to 15 feet	9 feet to 15 feet	9 feet to 15 feet
All other Residential Floors**	9 feet to 12 feet	9 feet to 12 feet	9 feet to 12 feet

*includes residential floor in Building #2 that fronts onto the green roof of the parking deck. If the green roof is not built, this floor shall be treated as all other residential floors.

**includes Non-Ground floor Live-Work units

The mass of the buildings are determined by a combination of height, building

length, stepbacks and bay spacing and upper cornice relation to the sky. For the purposes of establishing height and mass, the proposed buildings shall exhibit characteristics of height, step back, and mass within the expressed building footprint that are consistent with the parameters set forth in this section. The mass of the building(s) can be modulated by the use of roof forms and parapet treatments, materials and colors. It is highly recommended that the lower floors of the buildings (below the first stepback) be masonry, while the upper floors (above first stepback) be expressed in lighter, more transparent materials, thereby reducing the apparent mass of the buildings.

9.3.5 NJ Transit Construction Standards

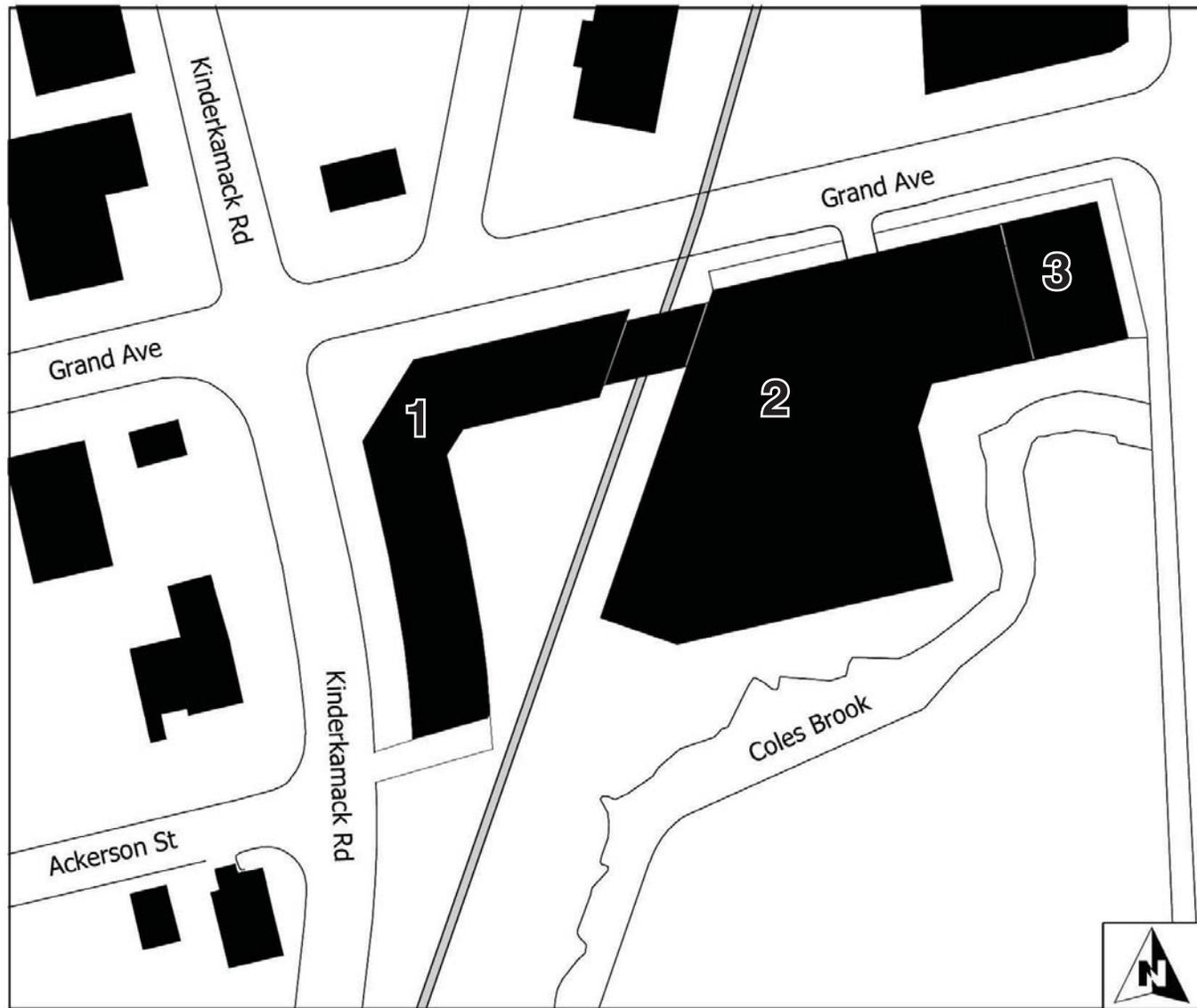
Along the Pascack Valley Rail Line, a minimum sixty (60) foot wide ROW must be maintained. In addition, all in-field site activities impacting NJ Transit operations must be coordinated with and approved by NJ Transit's Real Estate and Economic Development and Rail Operations units.

9.3.6 Residential Density

Each redevelopment project may be conditioned upon a negotiated maximum residential density, to be negotiated between the redevelopment entity and the designated redeveloper. The maximum permitted residential density shall be specifically set forth in an executed redeveloper's agreement prior to a development application being deemed complete for review before the Planning Board. The maximum permitted residential density shall include all housing units affordable to low- and moderate income household that are provided in accordance with this plan's provisions relating to affordable housing.

9.4 EXHIBITS OF BUILDING REGULATIONS

The following pages set forth the development regulations for each building.



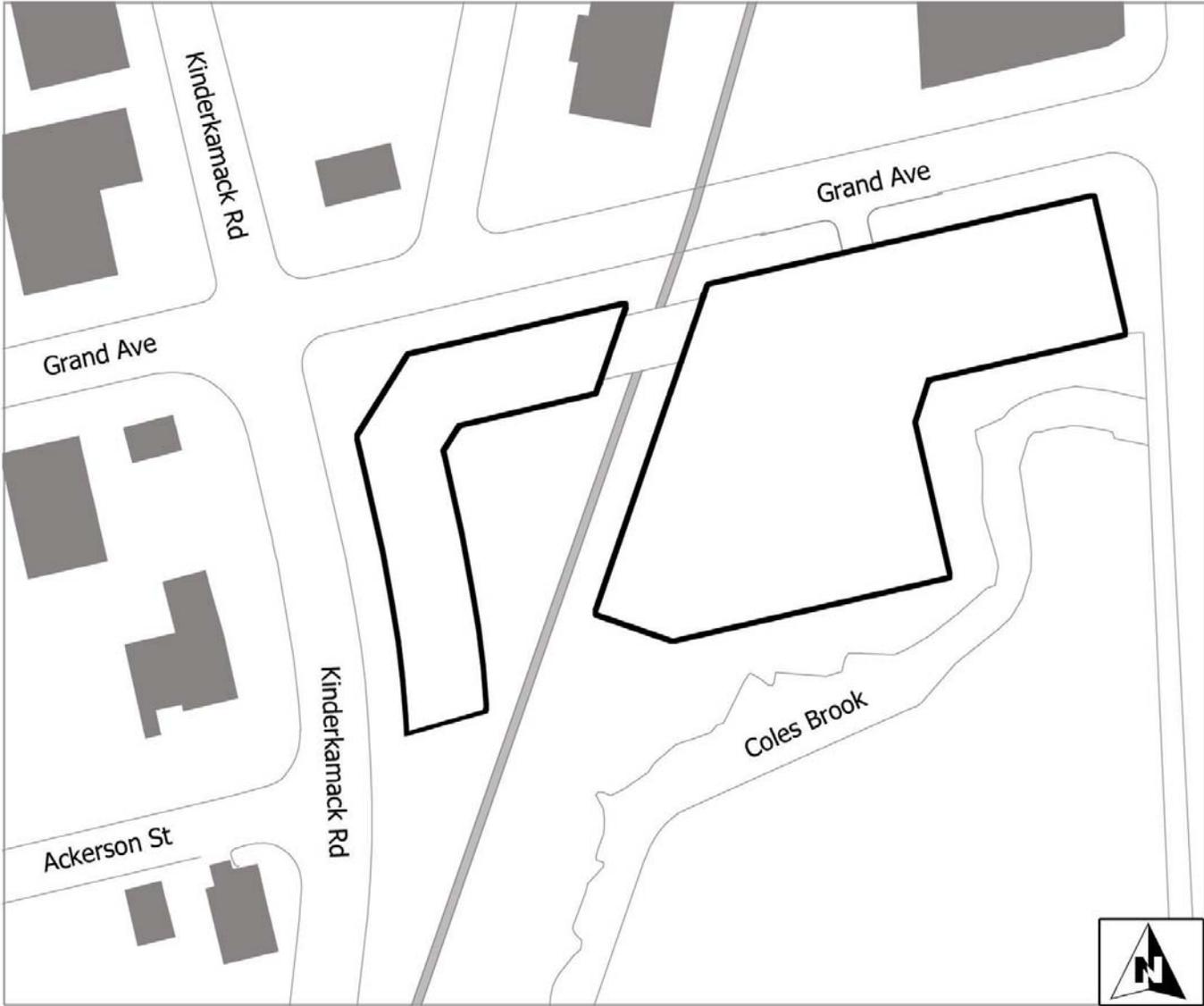


Exhibit 14.1 Building Regulations – Section AA (Building 1)

DETAILS	ID	INFORMATION												
SITE														
Section Number	x	AA												
Grade Change	GC	1.05% (Approximate)												
PUBLIC														
Parking	P	8 feet wide												
Sidewalk	SW	10 to 14 feet wide												
Planter Wall	PW	6 feet wide												
Elevated Sidewalk	ESW	10 to 14 feet wide (elevated 2 feet)												
Public Plaza	PP	100 feet (variable)												
BUILDING														
Building Type	n/a	Mixed-Use with Retail on Ground Floor												
Building (Depth)	B	60 feet												
Circulation 1	C1	10 feet												
Circulation 2	C2	n/a												
Building Height	BH	6 Stories, up to 74 feet (14 foot ground floor)												
Optional Building Height	OBH	6 Stories, up to 86 feet (14 foot ground floor)												
Stepback	ST	15 feet												
Rear Stepback	RST	10 feet												
Lower Encroachment	LE	Awning 8 feet, Balcony/Bay Window up to 3 feet												
Upper Encroachment	UE	Balcony up to 3 feet												
Rear Lower Encroachment	RLE	n/a												
Rear Upper Encroachment	RUE	n/a </tr <tr> <td>Special Consideration</td> <td>SC</td> <td>n/a</td> </tr> <tr> <td>Embedded Parking</td> <td>EPA</td> <td>n/a</td> </tr> <tr> <td>Rear Parking Encroachment</td> <td>RPE</td> <td>n/a</td> </tr> <tr> <td>Green Roof</td> <td>GR</td> <td>Highly Recommended (low level plantings)</td> </tr>	Special Consideration	SC	n/a	Embedded Parking	EPA	n/a	Rear Parking Encroachment	RPE	n/a	Green Roof	GR	Highly Recommended (low level plantings)
Special Consideration	SC	n/a												
Embedded Parking	EPA	n/a												
Rear Parking Encroachment	RPE	n/a												
Green Roof	GR	Highly Recommended (low level plantings)												

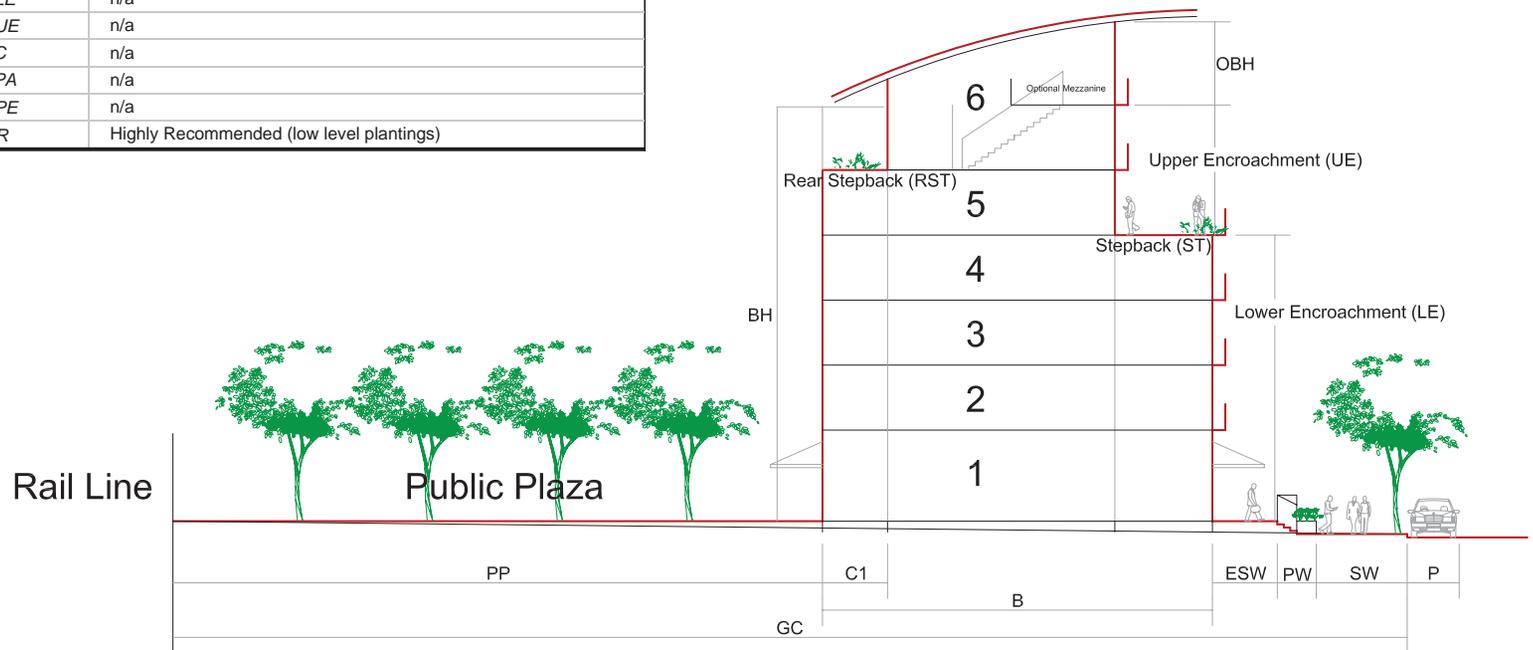
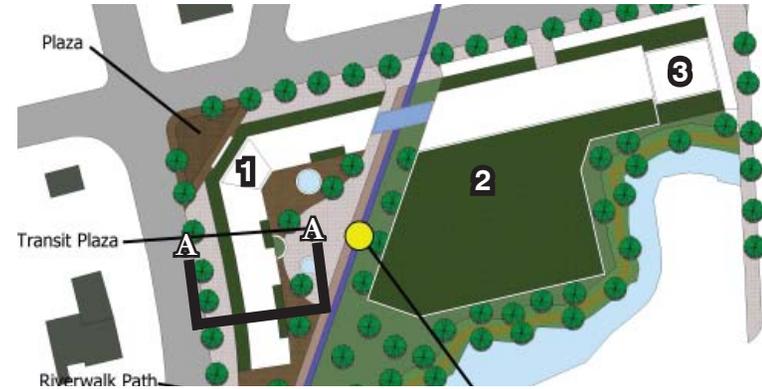


Exhibit 14.2 Building Regulations – Section BB Special Consideration (Building 2)

DETAILS	ID	INFORMATION
SITE		
Section Number	x	BB (Special Consideration)
Grade Change	GC	1.18% (Approximate)
PUBLIC		
Parking	P	8 feet
Sidewalk	SW	10 to 14 feet
Planter Wall	PW	6 feet
Elevated Sidewalk	ESW	10 to 14 feet (elevated 2 feet, as much as 4 feet)
Public Plaza	PP	195 feet (on top level of parking structure)
BUILDING		
Building Type	n/a	Mixed-Use with Office on Ground Floor
Building (Depth)	B	75 feet with 30 foot liner building adjacent
Circulation 1	C1	10 feet
Circulation 2	C2	5 feet
Building Height	BH	9 Stories, up to 110 feet with penthouse (10 foot ground floor)
Optional Building Height	OBH	n/a
Stepback	ST	15 feet (with plantings)
Rear Stepback	RST	10 feet (with plantings)
Lower Encroachment	LE	Balcony/Bay Window up to 3 feet
Upper Encroachment	UE	Balcony up to 3 feet
Rear Lower Encroachment	RLE	n/a
Rear Upper Encroachment	RUE	Balcony up to 3 feet
Special Consideration	SC	See Diagram
Embedded Parking	EPA	6 Levels @ 240 feet (variable, see plan view)
Rear Parking Encroachment	RPE	Optional balcony up to 3 feet
Green Roof	GR	Highly Recommended (low level plantings)

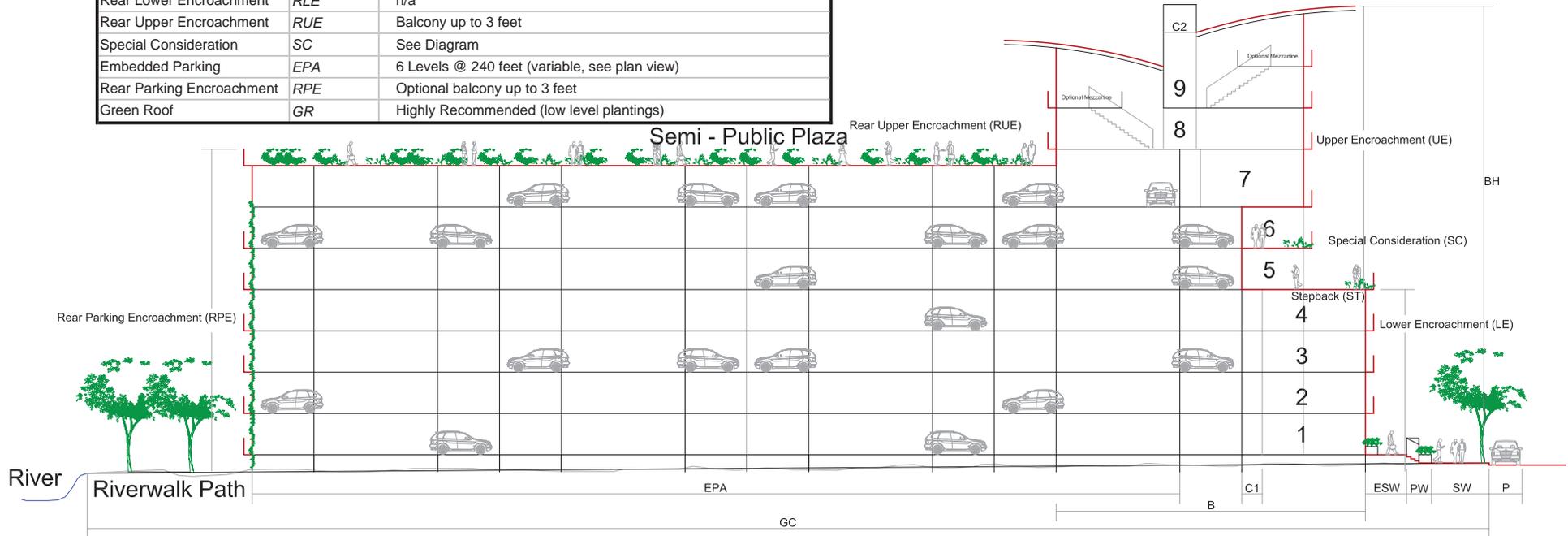
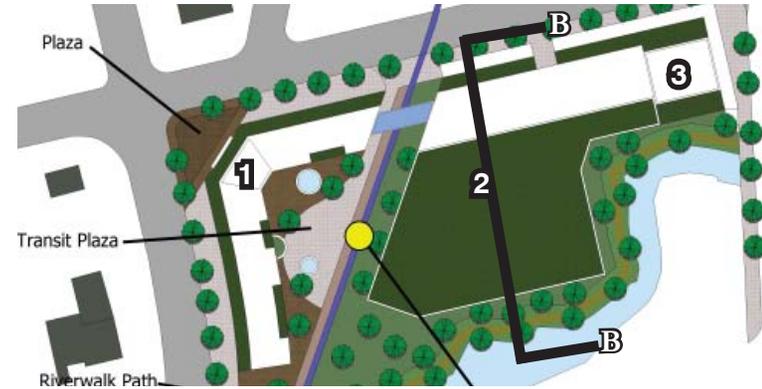


Exhibit 14.3 Building Regulations – Section BB (Building 2)

DETAILS	ID	INFORMATION
SITE		
Section Number	x	BB
Grade Change	GC	1.18% (Approximate)
PUBLIC		
Parking	P	8 feet wide
Sidewalk	SW	10 to 14 feet wide
Planter Wall	PW	6 feet wide
Elevated Sidewalk	ESW	10 to 14 feet wide (elevated 2 feet, as much as 4 feet)
Public Plaza	PP	195 feet (on top level of parking structure)
BUILDING		
Building Type	n/a	Mixed-Use with Office on Ground Floor
Building (Depth)	B	75 feet with 30 foot liner building adjacent
Circulation 1	C1	8 feet
Circulation 2	C2	5 feet
Building Height	BH	9 Stories, up to 110 feet with penthouse (10 foot ground floor)
Optional Building Height	OBH	n/a
Stepback	ST	15 feet
Rear Stepback	RST	10 feet
Lower Encroachment	LE	Balcony/Bay Window up to 3 feet
Upper Encroachment	UE	Balcony up to 3 feet
Rear Lower Encroachment	RLE	n/a
Rear Upper Encroachment	RUE	Balcony up to 3 feet
Special Consideration	SC	n/a
Embedded Parking	EPA	6 Levels @ 240 feet (variable, see plan view)
Rear Parking Encroachment	RPE	Optional balcony up to 3 feet
Green Roof	GR	Highly Recommended (low level plantings)

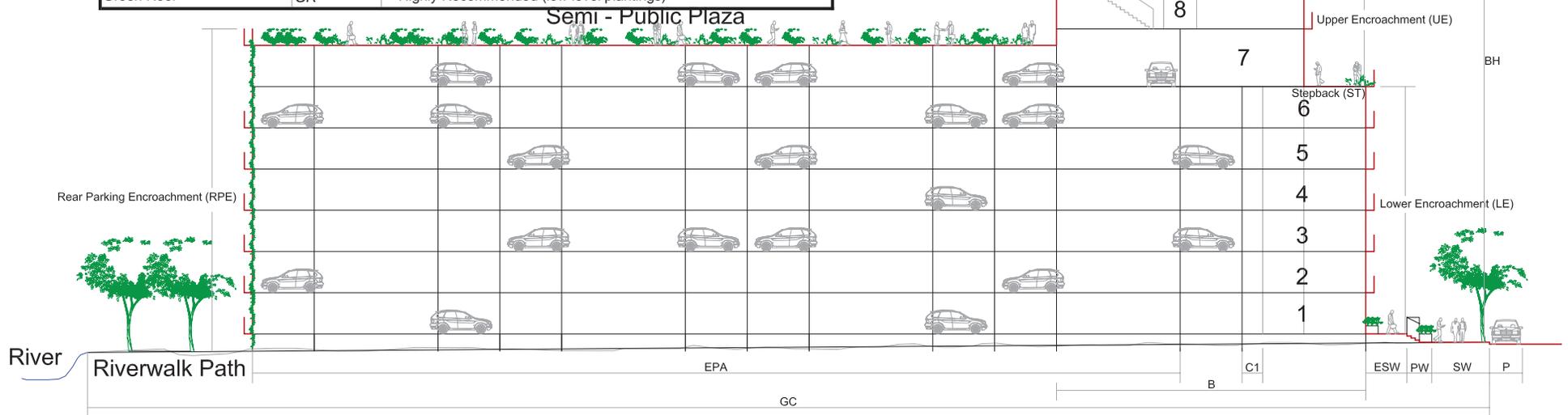
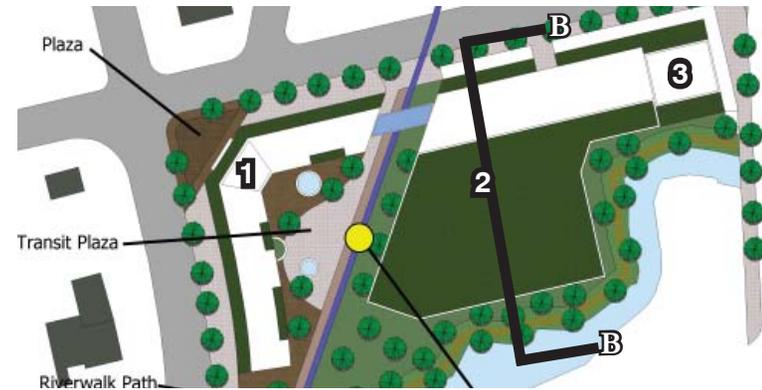
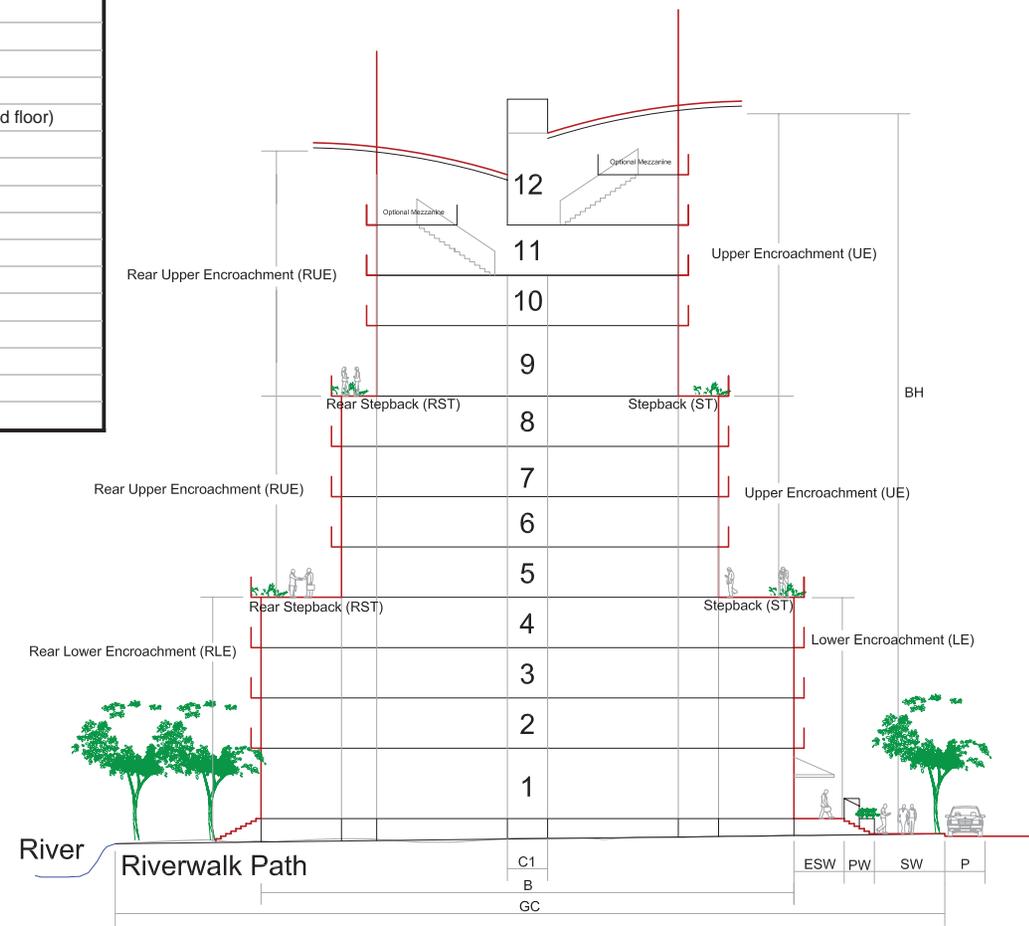
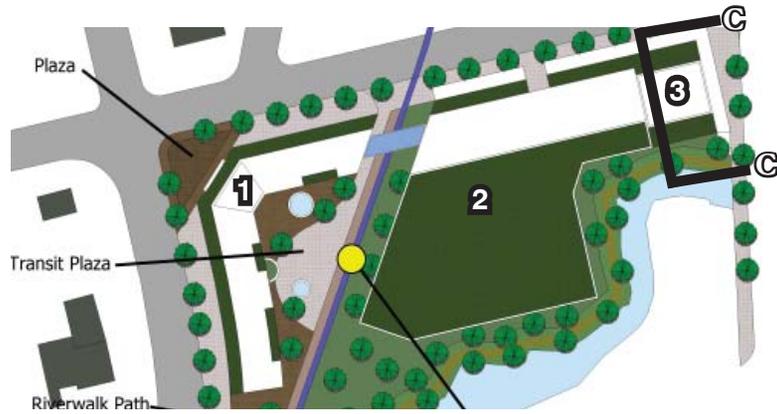


Exhibit 14.4 Building Regulations – Section CC (Building 3)

DETAILS	ID	INFORMATION
SITE		
Section Number	x	CC
Grade Change	GC	1.21% (Approximate)
PUBLIC		
Parking	P	8 feet wide
Sidewalk	SW	10 to 14 feet wide
Planter Wall	PW	6 feet wide
Elevated Sidewalk	ESW	10 to 14 feet wide (elevated 3 feet, as much as 4 feet)
Public Plaza	PP	n/a
BUILDING		
Building Type	n/a	Mixed-Use with Retail on Ground Floor
Building (Depth)	B	106 feet
Circulation 1	C1	8 feet
Circulation 2	C2	n/a
Building Height	BH	12 Stories, up to 143 feet with penthouse (14 foot ground floor)
Optional Building Height	OBH	n/a
Stepback	ST	15 feet
Rear Stepback	RST	10 feet
Lower Encroachment	LE	Balcony/Bay Window up to 3 feet
Upper Encroachment	UE	Balcony up to 3 feet
Rear Lower Encroachment	RLE	Balcony up to 3 feet
Rear Upper Encroachment	RUE	Balcony up to 3 feet
Special Consideration	SC	n/a
Embedded Parking	EPA	n/a
Rear Parking Encroachment	RPE	n/a
Green Roof	GR	Highly Recommended (low level plantings)



PART 2 - IF THIS PAGE IS PRINTED

DISCARD IT

DESIGN STANDARDS

10.1 MANDATORY ARCHITECTURAL STANDARDS

The following standards shall apply to all buildings to be constructed in the Area. To the extent that the development of any parcel is to be undertaken in phases, the mandatory or permitted standards for each building applies to each phase and must conform to the design standards provided in this Redevelopment Plan.

10.1.1 Architectural Review by Borough

The Buildings' proposed design and associated street improvements are subject to review by the Borough to insure the appropriate historical perspective, as envisioned by the New Bridge Landing State Park, is represented and accommodated to the greatest extent feasible.

10.1.2 Identifiable Building Width (Bay Spacing)

Building facades longer than 30 feet must be divided into identifiable building widths (IBW) with the building widths further divided into bays. An identifiable building width applies to long facades that must be designed to look like a series of individual buildings next to each other. The bay is a subdivision of the distance between structural elements of an IBW reflected on the facade. See **Exhibit 16.0**. Traditional town center type buildings are narrow, ranging from 15 to 50 feet. Generally, the entire facade of one of these narrow buildings is one identifiable building width, with that facade subdivided into bays. Typically, on historic buildings, the facade is a unique and complete composition by itself with a base, middle or top. The emphasis is on verticality. This is required on all new facades visible from a street or public space.

The facade of the parking structure adjacent to the Coles Brook must have an attached screening/trellis with plant species that will mask the facade in green.

The street-wall visual character is most positive when separate buildings are immediately adjacent to each other, with a similar build-to-line and unique architectural character. The entire composition of the street-wall based on these design features and the lower floor stepbacks will create a very human scale that is visually interesting. The buildings in this plan must reflect this character. In classic downtown areas, the IBW spacing ranges from 15 to approximately 50 feet wide. These bays historically were individual buildings with separate structural side walls. Each of these buildings had a coherent facade treatment from the ground to the upper cornice or parapet, expressing its' individuality through some combination

of the size and rhythm of the windows (vertical), variation in the height, color and pattern of the brick work, and the treatment of the cornice and the roof. When placed next to each other and sharing a build-to-line they create a harmonious whole, while still reflecting a level of individuality. This is a fundamental urban design characteristic which must be respected in the Area. When mixed-use residential buildings deviate significantly from the basic IBW module and a more consistent facade treatment is used, the building takes on an institutional look that lacks human scale and character for a town center. A modern building can emulate these standards as still look and function as a modern building.

Modern buildings are typically larger in scale and mass than historic buildings. Therefore to create a compatibility with the IBW rhythm of the traditional streetscape, the expression of the IBW must be apparent in the architecture of the proposed buildings. Emphasis must be on verticality not on horizontal features.

Each IBW with its combination of bays shall be expressed in the color of the materials and textures that create a vertical harmony. No long horizontal string courses are allowed.

Building #3 is not subject to this regulation (10.1.2)

10.1.3 Landmarks

Taller buildings and architectural features on corners of important buildings or at major locations of visual termination, act as landmarks providing legibility and interest to a street, plaza or walking experience. The corner treatment of any building in this Area must meet specified standards. Based on their location, they are illustrated as major and minor architectural landmarks as illustrated on **Exhibit 15.0**. They must have additional height or embellishments and must be contained within the specified areas illustrated. Multifaceted roof shapes for the corners designated as landmarks are required. Regulations providing a building envelope for landmark features are provided in **Exhibit 16.0**.

Building #3 is not subject to this regulation (10.1.3)

10.1.4 Encroachments

An encroachment is an area beyond the build-to-line into which certain building elements can protrude. The encroachment is expressed in feet by type allowed. Additionally, the story at which encroachments are allowed varies by the nature of the building edge. There are multiple building edges that are specified for each building face. See **Exhibits 14.1-14.4**.

10.1.5 Cornices

Cornices are required to emphasize the identifiable building widths or a combination of bays. At each stepback a decorative cornice is required. The decorative cornice will be combined with a parapet at each stepback or terrace level. The parapet height from the inside from the floor to the top of the cornice shall be a minimum of three (3) feet. Each cornice shall be distinctive along the edge of the stepbacks as well as the upper cornice at the skyline to reflect the identifiable building spacing from the ground level to the skyline. Long building walls with flat roofs must appear as a series of individual side by side buildings, each with distinct cornices.

10.1.6 Roof Lines

The tops/roofs of buildings must have a range of expressions. The tops of buildings are important design elements for the character of a building(s). To enhance the visual and spatial qualities of the buildings, variation of roof lines is required. A combination of flat roof with decorative cornice/parapet with difference step back lines and occasionally a pitched roof will modify the mass and scale of buildings, creating variety and visual delight.

10.1.7 Curved Roofs

Curved roofs are allowed. Standards for curved roofs are illustrated in **Exhibits 14.1-14.4**. If used these roofs must be high enough to be seen from the ground. The roofing material must have texture and color to complement the material of the body of the building.

10.1.8 Terraces and Green Roofs

Terraces created at the stepbacks must allow landscaping for outdoor use of adjacent units. Pergolas are highly recommended along with facilities for drip irrigation. Terraces must be used for outdoor use (dining, relaxation) and not storage.

Green Roofs are defined in *Section 6.0*. Green roofs are recommended on all flat roofs that can be seen from housing units or offices for individual and collective use of residents of the building. Green roofs are optional on the upper roof level. Green roofs provide reduced costs for cooling, increased insulation, less runoff, reduction of urban heat, and enhancement of urban living. These roofs shall be extensive (light shrubbery not to be walked on) or intensive (landscaping that can be walked and played on). It is highly recommended that the roof of the parking deck (building

#2) be intensively landscaped to improve the opportunity for recreation and overall quality of life for the residents of buildings #2 and #3. It is further recommended that, considering the size of the roof, tennis courts and other recreational uses be incorporated into this roof. See **Exhibit 19.0** for recommended locations and access points.

10.1.9 Solar

Solar collectors for the generation of electricity and or hot water are highly recommended on the roof tops. The collectors must be integrated into the architectural character of the building and must complement the overall design.

10.1.10 Ground Level Building Edges / Frontage Type

Building edges / frontages refer to the uses and architectural character of the first floor relationship to the ground plane. Certain building edges are adjacent to specific pedestrian realms, whereas others have adjacent green areas. See **Exhibits 17.1-17.9**.

In this plan there are nine (9) types of building edges / frontages that have adjacent pedestrian realms:

1. Shop front and Awning at grade (*Exhibit 17.1*)
2. Shop front and Awning on elevated sidewalk (*Exhibit 17.2*)
3. Elevated office frontage (*Exhibit 17.3*)
4. Elevated shop front and awning plaza at tower (*Exhibit 17.4*)
5. Shop front galleria (*Exhibit 17.5*)
6. Transit plaza with shop front and awning (*Exhibit 17.6*)
7. Parking structure and rail line (*Exhibit 17.7*)
8. Parking structure and riverwalk pathway (*Exhibit 17.8*)
9. Tower with stair and riverwalk pathway (*Exhibit 17.9*)

10.1.11 Galleria

A galleria is required to provide access to train station plaza from the street (shown in **Exhibit 17.5**). A galleria is a covered area for pedestrians with a consistent rhythm of columns supporting buildings above. The galleria is required to have the following characteristics:

- A. Gallerias shall be double height ranging from a minimum of 23 to a maximum 34 feet in height.

B. All retail fronts along the galleria shall provide a minimum of 80% glass. Storefront windows must provide maximum exposure to window displays, the inside of restaurants, or to other types of ground floor uses.

C. Columns and beams along the galleria shall have architectural features such as arches, plinths, and other visual features.

D. The architecture of the galleria can imitate historic designs but have features and surface patterns that visually reinforce the overall architectural design of the building.

E. The paving along the galleria must be visually distinct and continuous with the plaza. Brick, stone, marble, or other decorative and durable paving materials must be used.

F. If stairs are required due to the change in grade, no more than 3 steps may occur at a time, and a handicapped-access ramp must be provided.

10.1.12 Retail Frontages

Retail frontage is required in the locations specified in **Exhibits 8.1 and Exhibits 17.1-17.9**.

1. Entrances must be located at sidewalk grade level.
2. Retractable awnings shall be permitted to a minimum distance of 4 feet to a maximum distance of 8 feet from the building façade.
3. All retail fronts shall provide a minimum of 80% glass area. Storefront windows must provide maximum exposure to window displays, the inside of restaurants, or to other types of ground floor uses. To the extent possible, glass should start at grade or six inches above grade.
4. Shop fronts: The front of each store should be designed individually, with input from its tenant, rather than according to a repeated template.
5. Signing must meet standards set forth in Sections 10.4 and 10.5.
6. Further regulations are specified in **Exhibits 17.1-17.9**.

10.1.13 Residential Frontages

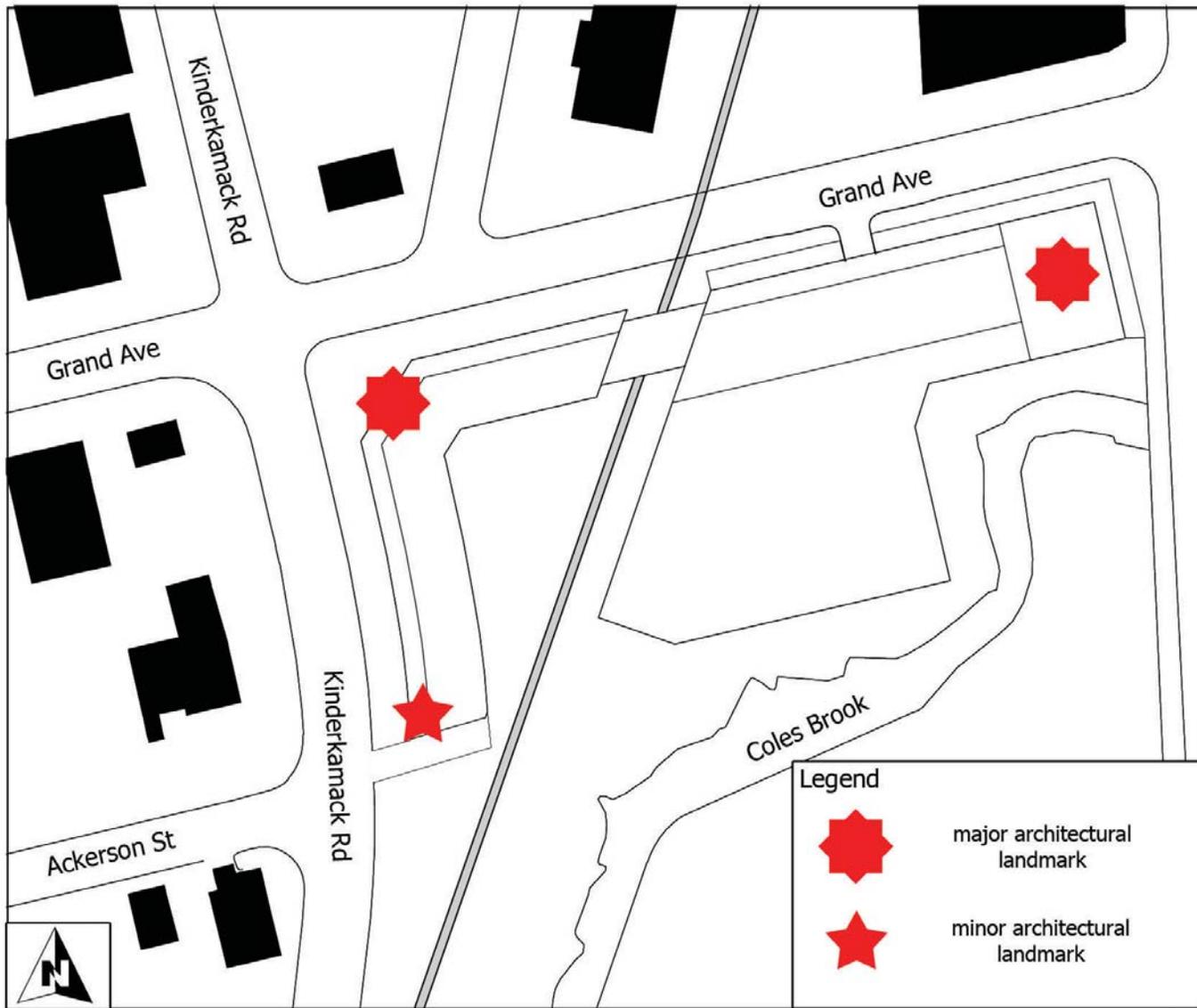
This section applies to those areas at the ground floor that provide access to residential on the upper floors. Recommended locations for residential entrances are shown on **Exhibit 9**. A large lobby must be provided for the residential Building #3 on the corner of Grand Avenue and Hackensack Avenue. Lobbies providing access to multiple units shall have access directly from sidewalk grade without steps, except where steps are needed to comply with flood levels. See **Exhibit 8.2**. Interior access to elevators must comply with ADA requirements. A canvas marquee can be used to emphasize the entrances. Marquees can extend to the curb edge.

10.1.14 Office Frontages

1. Offices entrances/lobbies can be located on frontages specified in Exhibit 17.3.
2. Offices are allowed on the first floor provided that a 4 foot semi-public landscaped edge is provided where windows overlook the street.

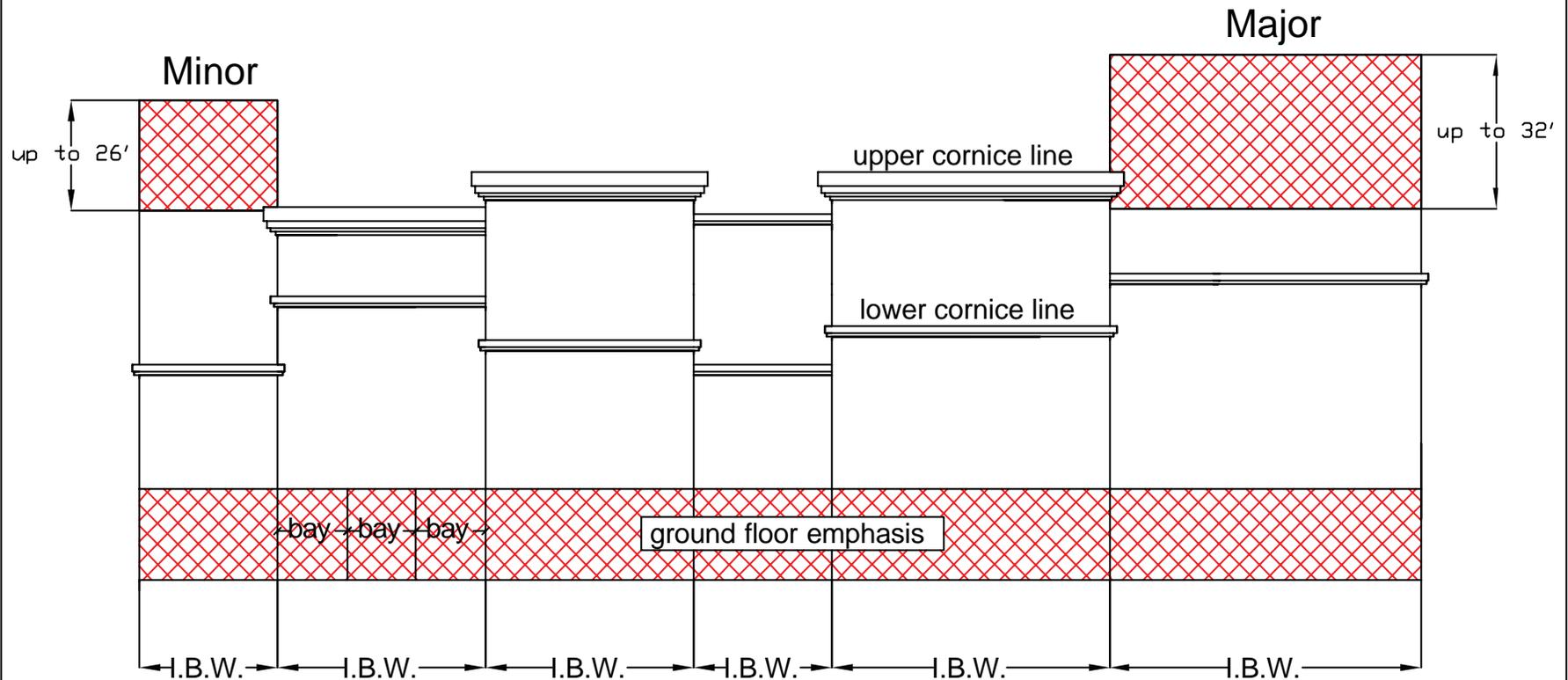
10.1.15 Parking Structures

1. Ingress/egress for the structure will interact with the pedestrian realm and shall be designed in such a way to maximize pedestrian safety, and minimize the pedestrian flow and disruption.
2. The exposed facades of parking structures shall either mimic residential façades by using vertically oriented openings that emulate residential window typologies or shall incorporate a metal lattice/screen on which vine type plants can grow. This second alternative is highly recommended along the Coles Brook edge of the parking structure. The more residential facade is required on the facade that edges the railroad and forms the building edge of the station plaza. See **Appendix 2.0** for acceptable examples.
3. Liner buildings are required along the Grand Avenue frontage of the parking structure and shall incorporate a mixed-use residential facade in form and character.
4. Decorative gates and fenestration must complement the architecture of the building facade.
5. Openings in the facades shall be designed to eliminate headlight lamp



Upper and lower cornice line must reflect I.B.W. spacing modules, which must range from 15' to 50'. Each I.B.W must be further subdivided with bays.

Major and minor architectural landmarks may extend up to 32' and 26' (respectively) above the upper cornice line.

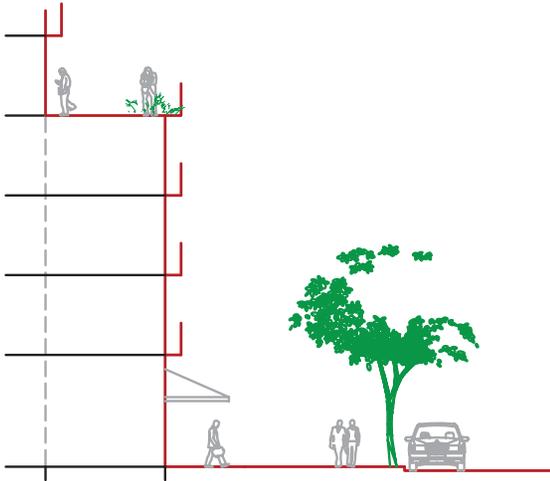


Note: this regulation does not apply to Building #3

Exhibit 17 Building Frontage Diagrams

Exhibit 17.1

Shop front and Awning at grade



Shop front and awning at grade features ground floor retail with awnings to protect pedestrians from rain and sun.

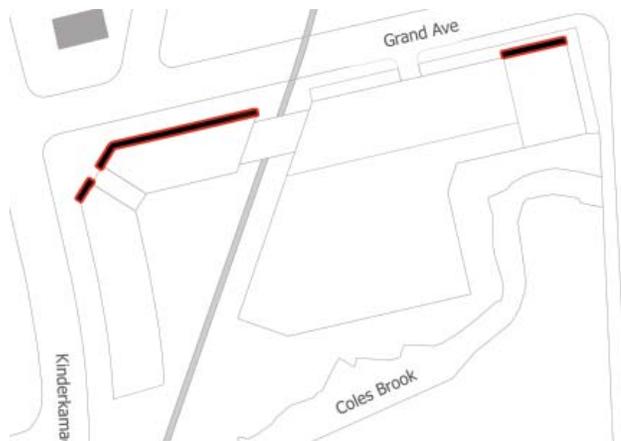
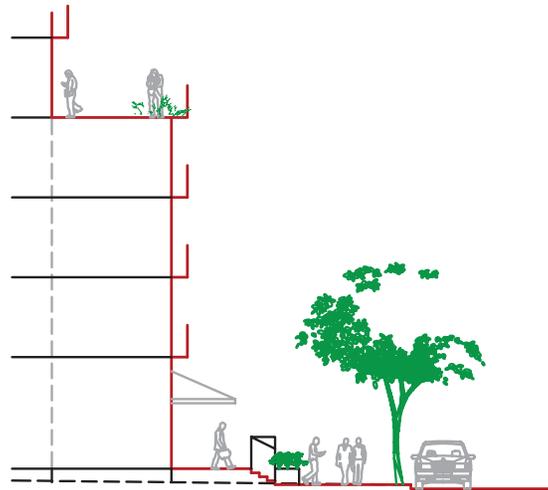


Exhibit 17.2

Shop front and Awning on elevated sidewalk



Shop front and awning on elevated sidewalk receives similar treatment as Exhibit 17.1 with the exception that due to grade change and 100yr floodplain elevation (See Section 9.2), the retail edge sidewalk must be elevated, connected to the lower (at grade) sidewalk by stairs and planters.

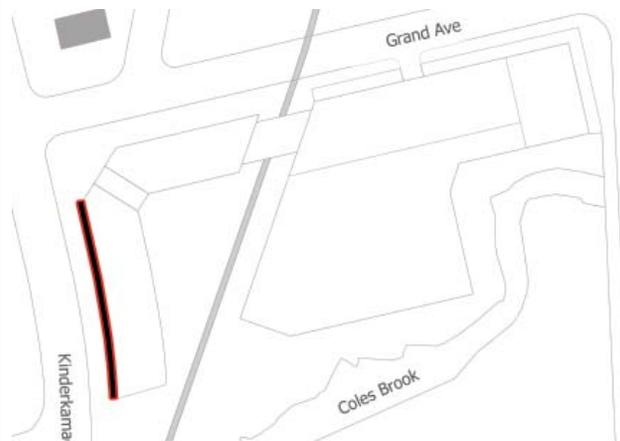
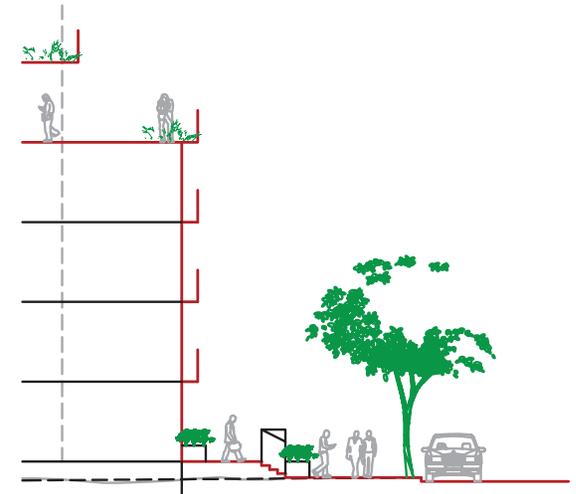


Exhibit 17.3

Elevated office frontage



Elevated office frontage also receives the elevated sidewalk treatment. Since the ground floor use is office, there is no awning. Instead there shall be a 2 foot to 5 foot planter at its edge.

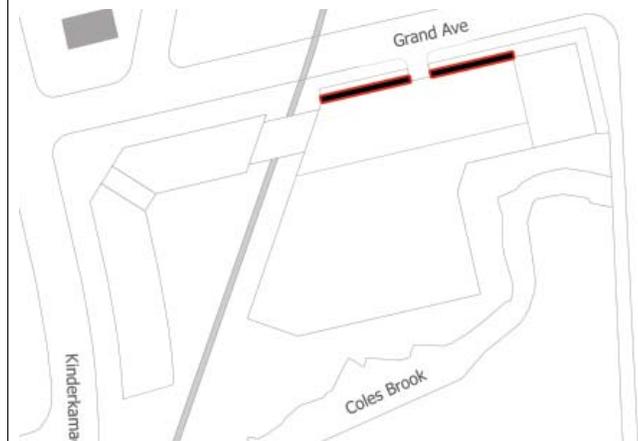
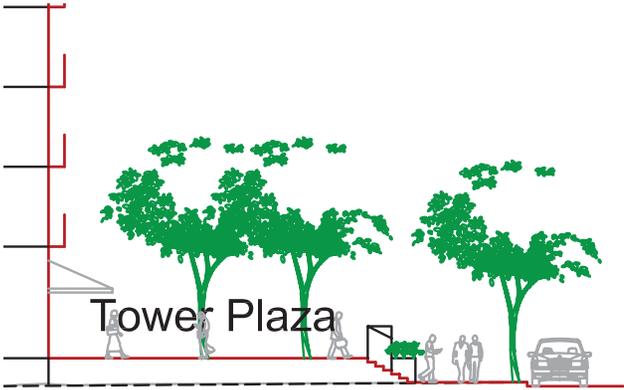


Exhibit 17 Building Frontage Diagrams (Cont'd)

Exhibit 17.4

Elevated shop front and awning plaza at tower



Along Hackensack Ave, there shall be an elevated plaza (according to Section 9.2) at the edge of the tower (building #3). This plaza shall be hardscaped, with plantings and trees pursuant to Appendix !.0.

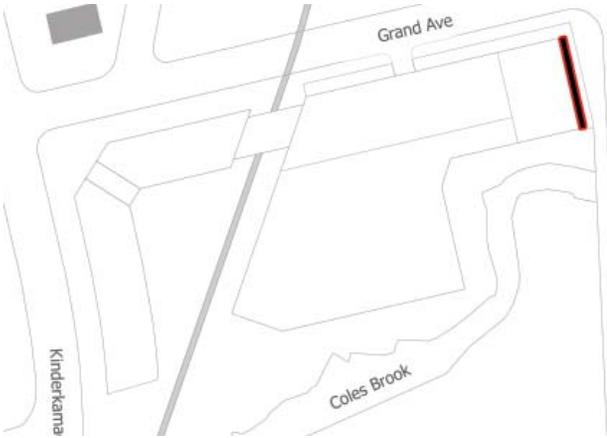
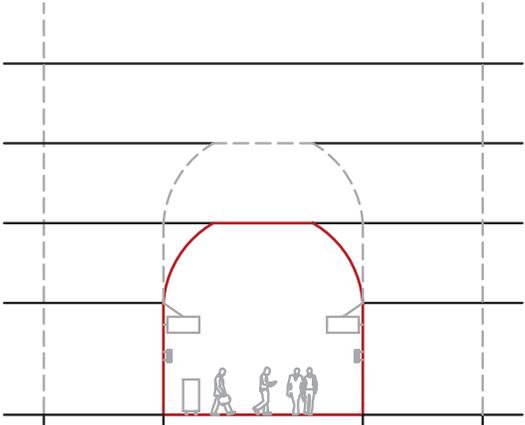


Exhibit 17.5

Shop front gallery



The gallery connects the pedestrian corner of Kinderkamack Rd and Grand Ave to the interior transit plaza. The retail uses that occupy the first floor in these locations should have display windows and an entrance along this edge. The gallery may be from 23-34 feet in height, although uses other than retail may not front onto the gallery.

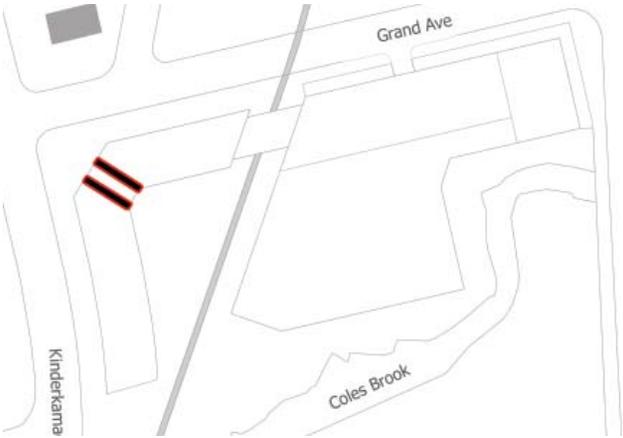
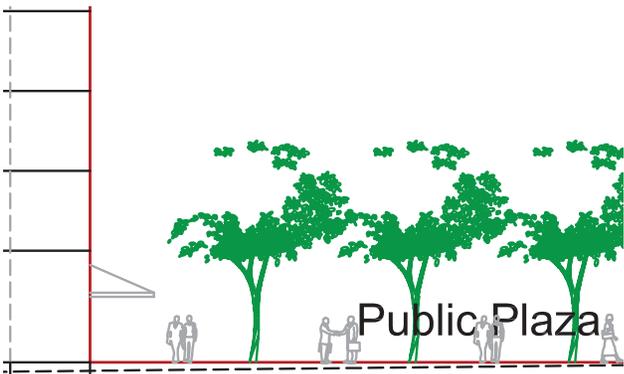


Exhibit 17.6

Transit plaza with shop front and awning



The transit plaza will have retail at the ground floor. This retail frontage shall have an awning.

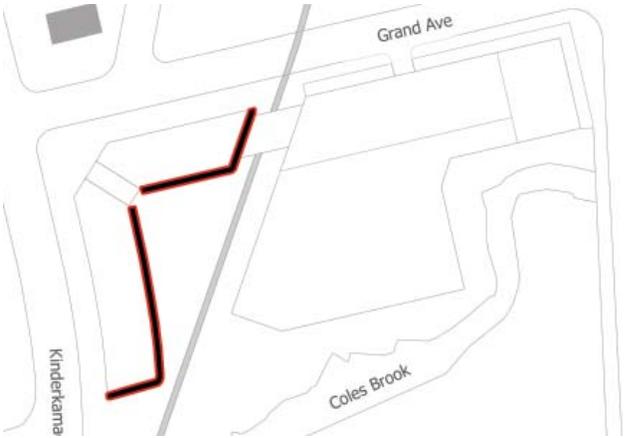
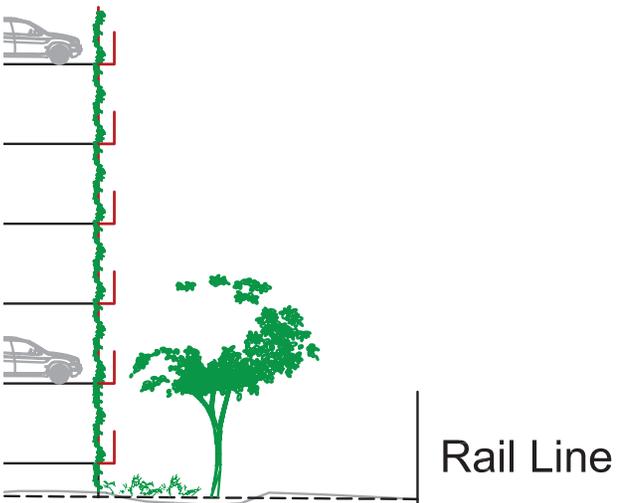


Exhibit 17 Building Frontage Diagrams (Cont'd)

Exhibit 17.7 Parking structure and rail line



Where the parking deck fronts onto the rail line, the edge shall be heavily landscaped. Fenestration in the parking deck walls shall have openings that emulate residential buildings in the Area.

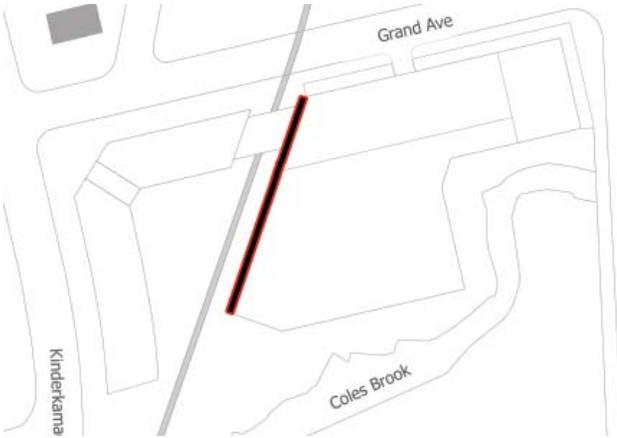
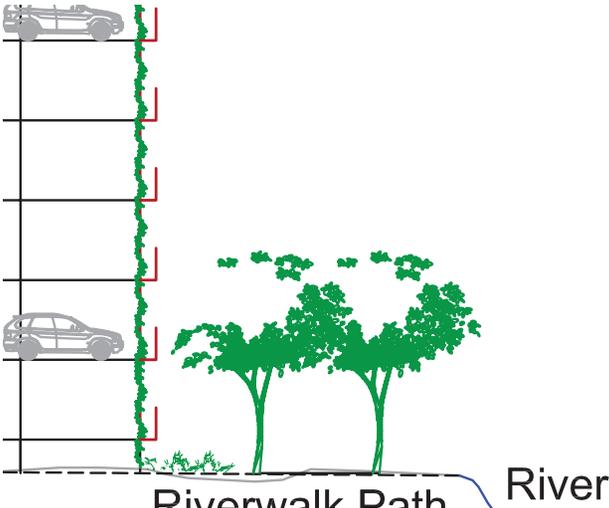


Exhibit 17.8 Parking structure and riverwalk pathway



Where the parking deck fronts onto the Cole's Brook, the edge shall be heavily landscaped. A riverwalk Path could run between a double row of trees. The wall of the parking deck shall be green faced, or screened with vegetation (see Appendix 2.0).

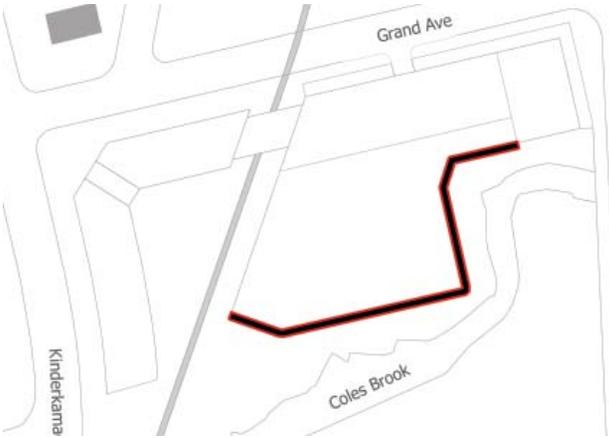
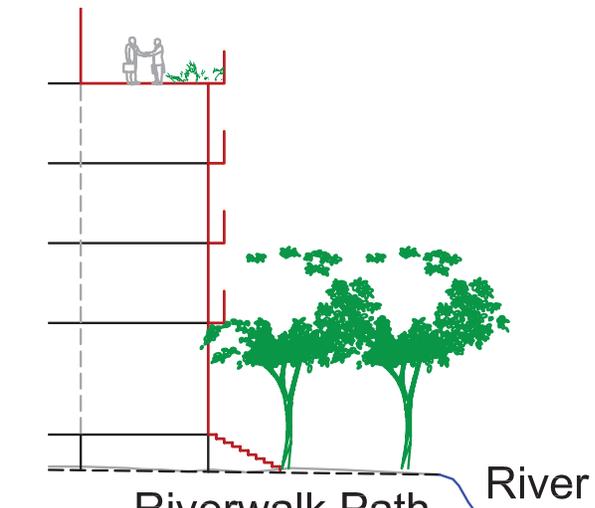
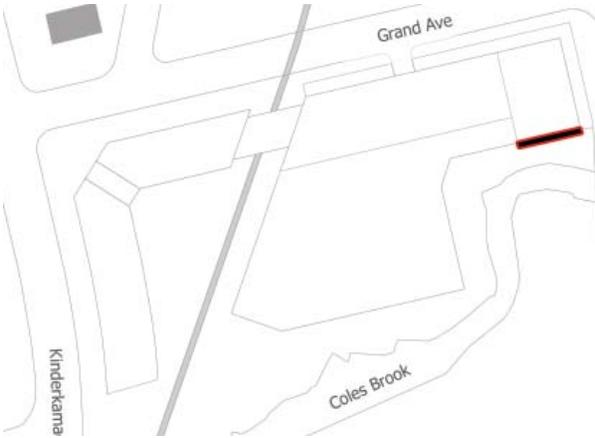


Exhibit 17.9 Tower with stair and riverwalk pathway



Where the Tower (building #3) fronts onto the Cole's Brook, there could be stairs leading down to the recreational path.



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glare from the inside to the outside of the structure.

6. Standards for parking decks are further illustrated in **Exhibit 14.2 and 14.3.**

10.2 ADDITIONAL ARCHITECTURAL STANDARDS

The standards previously stated in this Redevelopment Plan and the following required standards will insure that the proposed buildings are compatible with the positive images from the River Edge Town Center Visual Preference Survey and specifically illustrated in the Design Vocabulary while insuring the proposed buildings are “of its time.”

A. Buildings shall be designed with a horizontal subdivision that emphasizes the base, middle, and upper stories/cornice. Buildings shall have a vertical, not horizontal emphasis by use of IBW.

B. Buildings shall be designed to be aesthetically pleasing from all viewing points, using similar materials and fenestration on all facades except for the facade along Coles Brook.

C. Façade Materials: The primary facade materials shall be stone, masonry, brick, real stucco, glass and metal. No synthetic materials are allowed. Facades on Individual Building Widths (IBW) should consist of no more than three basic materials with a variety of textures and accents. A range of colors can be used to provide emphasis on the verticality of the individual building widths.

D. Façade Colors: A combination of lighter colored masonry/precast concrete and complementary darker brick in the earth tones shall be used at the lower facade (below first setback).

E. Variety: Buildings, particularly longer buildings fronting along Grand Avenue, should be designed in individual building widths expressed in different exterior materials or colors, roof treatment, cornice treatment, setbacks that create the appearance of having several smaller buildings arranged next to each other when viewed from the street. Each building within the redevelopment area can have a variation in architectural

expression thereby preventing the “institutional” look.

F. Each individual building width or combination of these along a streetwall must be expressed vertically from the ground level to the skyline, enhanced with decorative cornices at the setbacks and at the skyline. See **Exhibit 16.0.**

G. Rooftop mechanical equipment shall be screened in order to minimize the negative aesthetic impact upon the viewer both from street level and as may be viewed from surrounding buildings. Screening shall be consistent with the architectural design and materials of the building and may include higher parapets.

H. Mechanical structures shall be fully integrated with the architectural and structural design of the building.

I. All parts and components of cellular phone antennas, satellite dishes, and television and radio antennas and solar collectors shall be designed to be in harmony with the architectural context. Screening shall only be required in such cases where integration with the architecture of the building is not aesthetically responsible.

J. Building Frontages / Individual Building Width Spacing: All buildings are required to form an articulated building wall. Grand Avenue and Kinderkamack Road facades should be divided into individual building widths ranging from a minimum of 15 feet to maximum of 50 feet on center. Individual building widths must be expressed in changes in masonry color or pattern, modulation of the windows, variation in cornice design, setback between individual building widths etc. See **Exhibit 16.0.**

K. Building attachments: Awnings, projecting windows, roof overhangs, french balconies may encroach in front of the build-to-line, provided they conform to the standards set forth in the Encroachment Standards - See **Exhibit 14.** The maximum dimensions are listed below. Inclusion of any building attachment above those specified as encroachments shall be subject to the approval of the Planning Board. The maximum dimensions that may be so approved are as follows:

a. Awnings on the ground floor may extend to a maximum of 8 feet from the building facade and may not place supports upon the

SECTION 10.0 DESIGN STANDARDS

public sidewalk except if it is a marquee.

b. Bay Windows may extend to a maximum of 3 feet from the building façade.

c. Projecting/cantilevered “french balconies” may extend to a maximum of 1 foot from the building façade.

d. Architectural expressions on street corners may project up to 4 feet past the build-to-line.

L. Story Heights: See **Section 9.3.4**.

M. All retail establishments within a building shall place their primary entrances at sidewalk grade and have their doors recessed so as not to swing out onto the public sidewalk.

N. Prominent Entries: Main building entries shall be at grade, or on the elevated sidewalk where necessary to be above the flood level, see **Exhibit 8.2** and be easily identifiable as such with prominent architectural features; they should not occur simply as voids between buildings. All buildings shall have entrances accessed directly from the public sidewalk. Building 3, the “tower” shall have a prominent entry featuring a large main lobby and entry to prominent retail elements.

O. Façade Ratio: The percentage of void area (windows and other openings) in a building façade shall be between 40% and 60% at the lower facade and between 60% and 95% at the upper facade. Street-level retail frontages shall not be less than 80%.

P. Façade Composition: “Scattered-window” facades shall not be allowed along any frontage facing a public right-of-way. Each such façade shall present a unified, rational composition.

Q. Façade Materials at Corners: At predominant building corners, the façade materials must be the same on both faces meeting at the corner. These materials must extend at least 15 feet from the corner on both faces.

R. A major landmark corner shall be located on the tower (building #3) at the intersection of Grand Avenue and Hackensack Avenue, as well as a major

landmark at the intersection of Grand Avenue and Kinderkamack Road (building #1) and a minor landmark at the termination point of building #1 (near the intersection of Kinderkamack Rd and Ackerson St). Corners have been specified as major or minor landmarks (**see Exhibit 15.0**). There must be a clear difference between each type. Such expression can include projections, towers, roof forms, height increases or other architectural appurtenances appropriate to the scale and architectural expression and style selected. Stepbacks may be eliminated at the corners. Major landmarks are located on the primary corners at the intersection of primary streets. The corner of Grand Avenue and Hackensack Avenue is the location that shall have a major architectural embellishment signaling the entry into the area. Minor landmarks are recommended on secondary corners along major streets.

S. Parking Entrances: Parking structure entrances shall not simply be gaps between buildings, but through vehicular openings in the frontage line wall of the building. The parking structures shall provide direct pedestrian access to sidewalks such that users may exit the parking facility without entering a building. All vertical circulation areas within a parking facility shall be lit in the daylight-incandescent range. Pedestrian entrance doors shall be of a complimentary architectural style as the building and be painted a complementary color of the building.

T. Interiors of parking garages shall be painted white or a light color with each floor clearly identified.

U. Antennas: Antennas of any type shall only be permitted on the exterior of any building with the approval of the Planning Board as to their size, shape and location. If allowed they must be properly screened.

V. Utilities: All machinery and the mechanical controls for same, including but not limited to transformers, junction boxes, lift stations, electrical meters, condensers, and signal boxes, shall be interior to the block and masked from frontages by building elements in a manner consistent with the design of the building, incorporating false windows and dispersed venting to maintain the window rhythm and building pattern design. A wall of venting for mechanical rooms shall not be permitted.

W. All buildings shall display the street address of the building and/or name

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of building such that it is clearly visible from the adjoining street right-of-way day and night.

X. Flat roofed buildings shall have parapets that vary in height by the bay module and have unique decorative cornice.

Y. Terraces at the stepback levels shall be extensively landscaped with paving materials, planters, pergolas etc.

Z. Windows above the first level should have a vertical orientation. Windows shall be framed using sill and lintel. Punched windows with no decorative edging are not allowed. Window frames should be colored. White window frames are not allowed. Strip windows should not be used; i.e., no window should have a width to height ratio of greater than 2:1 without a separation from the adjacent window formed by the main building façade material. This requirement should neither apply to windows used for ground floor retail purposes, nor to the top floor of any building or clerestories. Windows must be set back a minimum of 3 inches from the primary facade.

AA. Night security gates, grids, or any other security covering of windows is prohibited.

10.3 OPEN SPACE AND LANDSCAPE PLAN DESIGN REQUIREMENTS

The street landscaping improvements identified in this section are dependant upon the execution of a RAD by the Borough of River Edge. The actual construction improvements shall be funded by the RAD, which is described further in the Redevelopment Plan Implementation Section.

The provision of “green areas” to include street trees, ground cover, small parks/plazas, planters, terraces and green roofs are necessary components of in-town living. Street trees must be provided. Bulb-outs with sufficient area for tree planting is recommended along Grand Avenue and Kinderkamack Road. A small transit plaza with a water element is a required behind Buildings 1 and 2, in addition to the streetscape. The recommended and illustrative Landscape Plan is shown on **Exhibits 18.0 and 19.0.**

Prior to the commencement of construction within the redevelopment area, an overall landscape plan for the streets, sidewalks, crosswalks, arcades, planters,

green roofs, terraces and plazas shall be presented and approved by the Borough Engineer for the streetscapes (lights, paving, street trees) and the Planning Board for all other landscaping.

The aforementioned plan shall be prepared by an experienced landscape architect. It shall incorporate the rights of-way design parameters for landscape as specified in this Redevelopment Plan, while providing sufficient detail as to how other open areas in the plan will incorporate quality materials and plant types.

The following additional standards shall apply:

A. The Landscape Plan shall include the highest and best quality decorative design materials available. The Landscape Plan shall identify at minimum architectural pavers, decorative lighting, tree species, and decorative site furnishings, including all specific color and material selections.

B. Street trees and plaza trees must be a minimum of 4 inches in caliper and 16 feet high at the time of planting. All street trees types and sizes shall be recommended by a local arborist, nurseryman, or some other landscape architect designated by the Planning Board.

C. Street trees shall be planted in adequately sized tree wells, with appropriate watering and feeding techniques. The area around the tree can be planted with groundcover, covered with decorative grates, or paved with decorative pavers.

D. All landscape materials must have a two year maintenance guarantee.

E. All ground areas that are not paved shall be landscaped with shade trees, flowering trees, evergreen and deciduous shrubs, and perennials and bulbs. Only plant materials with proven resistance to the urban environment should be utilized. Such lists are created by the Community Forestry Council, a division of the New Jersey Division of Parks & Forestry, or the New Jersey Nursery & Landscape Association.

F. All plantings should be done following current horticultural practices for urban locations. A recommended list of those practices is set forth in Appendix 1.



Exhibit 19.0 Recommended Green Roof Location



10.4 STREETScape REQUIREMENTS

The standards set forth in this section shall be binding for streetscape development. The exact configuration to be utilized within each right-of-way shall be made by the Borough Engineer, subject to the approval of the Redevelopment Entity.

A. Streetscape Materials:

- a. Vehicular cartpaths shall be constructed of asphalt.
- b. All curbing shall be granite curbing or poured concrete colored French Grey and brushed. Asphalt curbing is expressly forbidden. Curbs shall be 6" height without horizontal lips.
- c. Sidewalks shall be constructed of textured paving materials or concrete with brick edges and dividers.
- d. Crosswalks shall be of similar material and color as the paved sidewalks.
- e. No more than three paving materials or colors should be used per block.
- f. The surface plaza and transit plaza shall be a combination of brick and field stone. Modular paving or stamped concrete are also acceptable pursuant to Planning Board review. **Appendix 2.0** illustrates acceptable examples.

B. Street Tree Planting: The street trees shall be regularly spaced in accordance with the specifications in **Exhibits 18.0**.

- a. Street trees shall be planted in tree wells with decorative grills. Where possible, tree wells shall be protected by a low fence.
- b. Street trees shall be a minimum of 4" caliper 16 feet high at the time of planting.
- c. Selection of tree types shall be made by the Planning Board and the local arborists.

C. Street Lighting: Street lights shall be placed at the outer edge of all sidewalks and shall be as follows:

- a. A single lighting standard design for posts and fixtures must be used throughout the redevelopment area, with the mass and size varying by location and street type. The fixture type can be modified for use outside or inside of the Galleria. Final approval of the fixture, pole type, and location will be made by the Planning Board.

b. Street Frontages: One lighting standard, not to exceed 15 feet in height, for every 25 linear feet of sidewalk, on average.

c. Corners: Lighting must be increased in height and coverage on corners. Lighting standards should be placed on all four corners of intersections in excess of two lanes

d. All lighting shall be shielded to minimize night sky and prevent direct glare into adjacent residential windows.

e. Where possible, light levels shall be controlled to reduce lighting levels to the lowest possible standard that still permits safe passage at such hours of the night and at such locations that have little or no pedestrian activity.

f. Adjustments to these requirements may be made relative to one another in response to the photometric specifications of the chosen light standards. Street lights shall be placed by beginning at corners (without blocking crosswalks) and then working inward towards the middle of the block. Streetlights should produce a spectrum in the daylight-incandescent range.

g. Key lighting of predominant architectural details is highly encouraged, as well as display lighting in display windows.\

i. the Riverwalk Path shall be uplit.

D. Street Signage: Wherever possible, public signage shall be consolidated and affixed to lampposts.

E. Obstructions: Trash receptacles, mailboxes, bicycle racks, shall be located at the outer edge of the sidewalk. Exceptions: Vending racks and sidewalk dining may encroach upon a portion of the sidewalk providing that a 4 foot clear aisle is maintained, not including the area reserved for street tree planting. Benches shall be placed against building walls or at street edge facing retail fronts.

F. Streets: The outer 3 feet of sidewalks shall be constructed of bricks or similar approved pavers set between flush mounted planting wells along Grand Avenue, Kinderkamack Avenue and Hackensack Avenue. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.

G. All other Borough of River Edge streetscape design standards, as provided in the Borough Code shall be adhered to.

10.5 SIGNAGE REQUIREMENTS

In addition to adhering to the general standards contained within the Borough of River Edge Code, the following signage requirements apply to all uses:

- A. All signage shall be subject to Site Plan review and approval by the Planning Board.
- B. No fluorescent or glowing paint is permitted for any signage within the Area.
- C. No signs or advertising devices that are rooftop mounted, intermittently illuminated, flashing, or moving are allowed. Signs that might be mistaken for traffic control devices are also prohibited.
- D. Information kiosks with tenant listings and directions are permitted with a maximum of 8 square feet of signage area. Advertising is prohibited on kiosks.
- E. Freestanding signs are prohibited with the exception of way-finding identification installed with the approval of the Borough of River Edge.
- G. Signage above the second story of any structure is prohibited.
- H. Residential: A lobby serving more than 20 units may have a sign naming the building which may not exceed twenty square feet. The style of the lettering must be in character with the architecture of the building. Signage at entrances to individual units is limited to a street number not to exceed one square foot.
- I. Retail: Every retail establishment is permitted one facade sign and one sign per sidewalk frontage. Façade signage on windows or above display windows shall not exceed 30 inches in height. Blade signage shall not exceed 18 inches in height, nor project more than 3 feet from the façade. One hanging sign not to exceed four square feet is allowed for each retail use under the arcade.
- J. To the extent possible, lighting levels for signage should be controllable allowing the level of intensity to be significantly reduced at times of low pedestrian or vehicular activity.

K. Window displays for goods and services provided by retail establishments are encouraged and shall be lit with appropriate display lighting throughout the entire night.

L. Other sign requirements based on the existing ordinance that are not in direct conflict with the standards set forth above.

VALIDITY OF THE PLAN

SECTION 11.0 VALIDITY OF THE PLAN

If any section, subsection, paragraph, division, subdivision, clause or provision of this Plan shall be deemed by the courts to be invalid, such adjudication shall only apply to the particular section, subsection, paragraph, division, subdivision, clause or provision in question, and the balance of the Plan shall be adjudged valid and effective.

OTHER PROVISIONS

SECTION 12.0 OTHER PROVISIONS

12.1 ZONING MAP REVISIONS

The Zoning Map of the Borough of River Edge is hereby amended and shall be revised to show boundaries of the “Area” defined in this plan and identify the district as “The New Bridge Landing Station Redevelopment Area” and all provisions of this plan shall apply.

12.2 PROJECT SIGNS AND ADVERTISING

In order to facilitate the overall redevelopment of The New Bridge Landing Station Redevelopment Area, and to encourage further investment in the surrounding area and the Borough of River Edge in general, all advertising, signage, and other promotion of the development and redevelopment of the “Area” shall contain references to the proposed project’s location in the Borough of River Edge, so as to promote the positive aspects of the project, the New Bridge Landing Station Redevelopment Area, and the Borough of River Edge.

12.3 AFFORDABLE HOUSING

All development within the redevelopment area must conform with the latest Council on Affordable Housing (COAH) Standards for the number, size and cost of affordable units. The need for affordable housing based on number of proposed units and new retail/commercial must be accommodated within each focus area or by agreement with the developer within the Redevelopment Area.

12.4 ENVIRONMENTAL REGULATIONS

All development within the redevelopment area must conform to the latest EPA and NJDEP environmental standards as of December 6, 2006.

PLAN CONSISTENCY REVIEW

As required by the Redevelopment and Housing Law, this section describes the consistency between the New Bridge Landing Station Redevelopment Plan and the Borough of River Edge's Master Plan and how the provisions herein are designed to effectuate the Master Plan. This Plan also describes the relationship of the New Bridge Landing Station Redevelopment Plan to the master plans of contiguous municipalities; the Bergen County Master Plan, and the New Jersey State Development and Redevelopment Plan.

13.1 RELATIONSHIP TO LOCAL OBJECTIVES OUTLINED IN THE BOROUGH OF RIVER EDGE MASTER PLAN (2003)

An analysis of the 1984 Borough of River Edge Master Plan and its 2003 Re-Examination Report did not reveal substantial inconsistencies with the goals and objectives of the New Bridge Landing Station Redevelopment Plan. The Re-Examination—chiefly, its recommendation for major mixed-use redevelopment of the southern sector of the municipality—lends support to Plan.

The New Bridge Landing Station Redevelopment Plan addresses many of the goals and objectives articulated in the 1984 Master Plan, including:

- Consideration of “the redevelopment of the area of town south of Main Street with a mixture of commercial and residential development.”
- Exploration of “development in and around the Train Station in the south end of town to exploit the available public transportation and nearby highways...”

Additionally, the “Recommended Changes” section of the 2003 Re-Examination Report lends support to the Redevelopment Plan:

- “[B]etter utilize the commercial district, specifically the area from Main Street south to the Hackensack border as many of the lots in this area are underutilized and do not constitute the highest and best use given the location.”
- “[E]xploit the proximity of the New Jersey Transit train station and develop mixed use development in the area.”
- “[A]ssure the highest and best and development of the area” by reconsidering building height and density requirements and re-evaluating permitted zoning uses.

The New Bridge Landing Station Redevelopment Plan, which creates a pedestrian-oriented, mixed-use streetscape, would help achieve the objectives of the Master Plan and its Re-Examination regarding the exploitation of regional transit and the appropriate mix of land uses in the area.

13.2 RELATIONSHIP WITH EXISTING ZONING ORDINANCE

Based on the Land Use Map in the 1984 Master Plan, the land within the Redevelopment Area is composed of three uses. The parcels west of the rail ROW is a surface parking lot serving the station, described as Public & Semi-Public use. Just east of the rail is a strip of vacant, wooded land. The eastern portion of the Area is a vacant commercial lot that is the site of a demolished car wash.

The Redevelopment Area falls entirely within the C2 Commercial zone, which includes business and professional offices, public buildings and uses, funeral parlors, banks and financial institutions, department stores and supermarkets, real estate and insurance offices, restaurants and churches and schools. The zoning code described in this Redevelopment Plan supersedes the zoning ordinance of the Borough.

13.3 EFFECT ON ADJOINING MUNICIPALITIES

Hackensack

The Redevelopment Area lies in southern River Edge, partially on the border of Hackensack. The parcels directly adjacent to the Area are office towers fronting Hackensack Avenue. These towers have large surface and structured parking lots that abut the Area. Route 4, Hackensack Avenue, and their associated interchanges create a large buffer around this area. On the eastern side of Hackensack Boulevard (across from the Redevelopment Area) are large industrial complexes with mass amounts of surface parking. The scale and use proposed by the Redevelopment Plan are not in conflict with those on these parcels. However, density and value added to the Redevelopment Area may spur consideration for redevelopment on these parcels as well.

Paramus

The southeastern edge of the Borough of Paramus comes within approximately one-quarter mile of the western border of the redevelopment area. Local connection is

SECTION 13.0 PLAN CONSISTENCY REVIEW

provided via Grand Avenue to NJ Route 4. Route 4 entering Paramus is lined with commercial and office uses. The Paramus Master Plan considers uses along both the border in both municipalities to be compatible, and makes no mention of River Edge redevelopment efforts. It is expected that residents of Paramus may take advantage of the improved train station area and parking facilities easily accessed from Route 4.

New Milford

New Milford is approximately one-third mile from the Area across the Hackensack River. The municipality is accessed most directly by Hackensack Avenue, which becomes New Bridge Road as it crosses the river. The transition from commercial/industrial uses to single family is buffered not only by the river and bridge but also by a stretch of tree lined road leading to and from the bridge on the River Edge side. No conflict between the Redevelopment plan and this municipality are anticipated.

Oradell

River Edge shares its northern border with the Borough of Oradell, over a mile north of the Redevelopment Area. The most direct route between the Area and Oradell is Kinderkamack Road. Because of the distance, there is no anticipated conflict or relationship between the Area and the Borough of Oradell.

13.4 CONSISTENCY WITH THE BERGEN COUNTY MASTER PLAN

The Bergen County Master Plan is currently in the process of being written. When it is completed, this Plan is subject to consistency review.

13.5 CONSISTENCY WITH THE NEW JERSEY STATE DEVELOPMENT AND REDEVELOPMENT PLAN (2001)

The New Bridge Landing Station Redevelopment Plan furthers many of the goals and objectives outlined in the New Jersey State Development and Redevelopment Plan. As a part of the Metropolitan Planning Area (PA1), the Redevelopment Plan is consistent with the State Plan's intent to promote growth in compact form. Because this area includes the train station, intensities of residential and commercial uses are expected to be sufficient to support transit, promoting pedestrianism and reducing dependence on the automobile.

13.6 CONSISTENCY WITH SMART GROWTH PRINCIPLES

The State of New Jersey Office of Smart Growth has promulgated principles of Smart Growth to which redevelopment plans must conform. The New Bridge Landing Station Redevelopment Plan is in maximum conformance with these principles. It is mixed use and compact, in a walkable neighborhood, offering a distinctive and attractive sense of place. It uses existing infrastructure and promotes transit use. It has reached out to the community in the conceptual design process.

ACQUISITION AND RELOCATION

SECTION 14.0 ACQUISITION AND RELOCATION

Since the properties concerned with this plan are owned by either the Borough or NJTransit, no acquisition or relocation is required. However, surface parking will be displaced. Therefore, it is suggested that Parcel B be built first so that the existing parking lot may continue to be used while the parking structure is constructed. Once the parking deck is constructed the surface parking can be relocated there, and Parcel A will be able to be built. An interim commuter parking plan must be developed and submitted to NJ Transit for approval prior to commencement of any phase of development that would result in relocation or reduction in existing NJ Transit commuter parking.

REDEVELOPMENT PLAN IMPLEMENTATION

SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

15.1 REDEVELOPMENT ENTITY

The Redevelopment Entity shall have such powers and duties as are set forth in New Jersey's Local Redevelopment and Housing Law (LRHL) and as may be conferred by this Redevelopment Plan, including but not limited to the authority to acquire real property, to relocate residents and businesses, to designate redevelopers, to establish clear terms and conditions for redevelopment through the negotiation, execution and administration of Redeveloper's Agreements, and to do such other things as provided by law.

15.1.1 Redevelopment Plan Requirements

The Redevelopment Entity will review concept plans provided by the redeveloper before submission to the Borough Planning Board. This process will help assure the Borough's redevelopment entity that the redeveloper is following the design standards appropriately.

15.2 PARCELIZATION

The diagrams, images and other graphic representations provided in this Redevelopment Plan are intended to provide a framework for interpretation of the written standards and regulations contained herein. Nothing in this Redevelopment Plan shall preclude the partial redevelopment of a block depicted in such diagrams, images or other graphic representations, provided that such subdivision or re-subdivision and partial redevelopment of a block is fully in conformance with the written standards and regulations contained herein. (Such parcelization of any disposition parcel shall be subject to approval by the Planning Board and shall be reflected within the Redeveloper Agreements as may be executed between the Redevelopment Entity and such Redevelopers as may be designated by the Redevelopment Entity.)

15.3 PLANNING BOARD REVIEW AND APPROVAL

A. The Planning Board shall have the right to review and approve a redeveloper's plans and specifications with respect to their conformance to this Redevelopment Plan. Such a review shall be on the basis of a site plan submitted to the Planning Board. No additional construction or alteration to existing or proposed constructions shall take place until a site plan reflecting such additional or revised construction has been submitted to and approved by the Planning Board. This pertains to revisions or additions prior to, during and after completion of the improvements.

B. Prior to commencement of: (a) any new construction, (b) reconstruction, (c)

rehabilitation, (d) any change in the use of any structure or parcel, or (e) any change in the intensity of use of any structure or parcel, a site plan for such shall be submitted by the redeveloper or property owner to the Planning Board for review and site plan approval. No building permit shall be issued for any work associated with items (a) through (e) above without prior site plan review and approval by the Planning Board.

Site plan review shall be conducted by the Borough of River Edge Planning Board pursuant to NJSA 40:55D-1 et seq. Site plan review shall consist of a preliminary site plan application and a final site plan application which may be heard concurrently. Submission of a site plan and site plan application shall conform to the requirements of this Plan. Applications may be submitted for individual buildings or groups of buildings, entire blocks or portions of a block, or a project group. Final site plan approval for any construction shall not be granted unless or until that construction is substantially complete, or performance guarantees for site improvements for that phase have been furnished by the redeveloper in accordance with NJSA 40:55D-53.

No Certificate of Occupancy of any type shall be issued for any construction until the Planning Board has given final site plan approval for the building, building group, block, or project group in which such construction is located. As part of the final site plan approval, the Planning Board may require a developer to furnish performance guarantees pursuant to NJSA 40:55D-53. Such performance guarantees shall be in favor of the Borough of River Edge and in a form approved by the Borough of River Edge.

The Planning Board at the time of site plan review and without formal amendment to this plan, may approve minor modifications from the standards set forth herein, if deemed to be in the interest of project implementation and in furtherance of this Plan and the standards set forth herein.

15.4 REVENUE ALLOCATION DISTRICT

RECOMMENDATION FOR THIS AREA TO BE INCORPORATED INTO A REVENUE ALLOCATION DISTRICT (RAD)

A. Tax Increment Financing and a RAD: Tax Increment Financing uses one or more of the following:

- 1) The incremental increase in property tax revenues created by

SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

redevelopment (the “Property Tax Increment”)

- 2) Incremental Payments in lieu of taxes (“PILOTS”) and/or
- 3) Parking revenue from public parking facilities to fund the debt service on municipal bonds that the municipality issues to finance a project, called the “Project”, in the RAD.

B. The planned developments in the New Bridge Landing Station District must be likely to be realized by private enterprise, but would not likely be accomplished by private enterprise without the creation of the RAD and the revenue allocation financing of the proposed Project or Projects.

C. Smart Growth: The creation of the RAD must be in compliance with smart growth planning principles. To this end, the State Planning Commission must provide Initial Plan Endorsement to all RADs pursuant to N.J.A.C. 5:85-7. “Smart Growth” means well-planned, well-managed growth that adds new homes, creates new jobs and promotes redevelopment and urban revitalization, while preserving open space, farmland, and environmental resources as set forth in the State Development and Redevelopment Plan.

D. Property Tax Increment: The Property Tax Increment for the District is calculated pursuant to the formula set forth in the definition of “Property tax increment” in N.J.S.A. 52:27D-461, which is as follows:

[General Tax Rate of the municipality For The Current Tax Year x The Taxable Value Of All Property Assessed Within The District In That Year, excluding any special assessments] x [(Taxable Value of All Property in the District - the aggregate taxable value of all property assessed which is located within the District as of October 1 of the year preceding the year in which the District is authorized) ÷ Taxable Value Of All Property Assessed Within The District] = Property Tax Increment

E. Sufficiency of Pledged Funds: The Property Tax Increment of the RAD, PILOTS from the RAD and/or parking revenue from public parking facilities built as part of the Project must be sufficient to meet the debt service (principal and interest payments) on the Bonds to finance the Project.

F. Public Project for Tax Exempt Bonds: In order for the interest payments on the municipal bonds to be exempt from Federal Income Taxation the Project must be a public project.

G. Municipal Guaranty of the Bonds: While not legally required, in order for the bonds to have a high rating and a good interest rate, the municipality must guaranty their repayment.

- H. Four (4) Step Process to Create a RAD and Issue Bonds Pursuant Thereto:
1. RAD Plan Assessment,
 2. Application for approval to create a Revenue Allocation District and Preliminary Revenue Allocation Plan,
 3. Approval of a Final Revenue Allocation Plan, and
 4. Approval of a financial instrument.

Anticipated redevelopment projects and activities for which the use of RAD revenues anticipated is:

- a. Acquisition of private properties
- b. Construction of new or realigned streets;
- c. Public improvements, inclusive of:
 1. Pedestrian walkways and public plazas;
 2. Lighting, streetscape, and landscape improvements;
 3. Other public improvements determined to be necessary or desirable for the effective implementation of the Redevelopment Plan.

15.5 NJ TRANSIT REVIEW AND APPROVAL OF REDEVELOPMENT PLAN IMPLEMENTATION

Developers will be required to negotiate with NJ Transit for the use and/or redevelopment of all NJ Transit owned lands within the redevelopment area, independent of the Borough and subject to NJ Transit Board Approval.

15.5.1 Reimbursement for Borough Costs

A designated developer shall be required to pay all applicable escrow fees and other required charges for municipal professionals in a manner similar to the applicable provisions of the Borough’s Site Plan Ordinance and New Jersey

SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

statutes as in the case of Planning Board review. In addition, a redeveloper shall be required to pay its proportional share of the costs of any studies, plans, reports, fiscal impact studies, or analyses prepared by the Borough or its designated redevelopment entity as part of the implementation of the Redevelopment Plan. Any such payments required to reimburse the Borough shall be specified in the Redevelopment Agreement and an interim cost sharing agreement effective prior to the Redevelopment Agreement.

Redevelopers further will be required to reimburse the Borough for all of its costs in the preparation of this Redevelopment Plan in proportion to the intensity of development permitted on the parcel being developed by that Redeveloper.

15.6 FINANCIAL INCENTIVES

The Borough of River Edge may use its redevelopment powers pursuant to the LHRL to provide financing for any aspect of the Plan including (1) direct financing secured by revenues from any portion of the project undertaken pursuant to the Plan or by the general taxing power of the Borough; or (2) making of any loans to the designated redeveloper; or (3) the extension of credit to the designated redeveloper in such form as the Borough of River Edge determines is appropriate. The extension of credit shall not include a guarantee by the Borough of River Edge of any debt.

15.7 NON-DISCRIMINATION PROVISION

Please Take Notice no covenant, lease, conveyance or other instrument shall be affected or executed by the Borough Council of the Borough of River Edge or by a developer or any of its successors or assigns, whereby land within the redevelopment area is restricted by the Borough Council of the Borough of River Edge, or the developer, upon the basis of race, age, creed, color, handicap, religion, or national origin in the sale, lease, use or occupancy thereof. There shall be no restrictions to occupancy or use of any part of the redevelopment area on the basis of race, age, creed, color, handicap, religion, or national origin.

SCHEDULE AND DURATION OF THE PLAN

SECTION 16.0 SCHEDULE AND DURATION OF THE PLAN

16.1 SCHEDULE FOR PERFORMANCE

Any Redeveloper Agreement executed for purposes of implementing this Redevelopment Plan shall contain a Schedule for Performance for the construction of the improvements covered by such agreement. In addition to a schedule for commencement of improvements, such agreements shall include a schedule that indicates the approximate time period for property acquisition, development approvals, construction permits, relocation, etc. required prior to construction as well as a projected date for project completion. The Redeveloper Agreement shall also set forth the interim uses for the property and the duration of the period during which such interim uses will be in place.

16.2 DURATION OF THE PLAN

The provisions of this Plan specifying the redevelopment of the project area and the requirements and restriction with respect thereto shall be in effect for a period of fifty (50) years from the date of approval of this Plan by the Municipal Council of the Borough of River Edge.

16.3 CERTIFICATES OF COMPLETION AND COMPLIANCE

Upon the inspection and verification by the Redevelopment Entity that the redevelopment of a parcel subject to a Redevelopers Agreement has been completed, a Certificate of Completion and Compliance shall be issued to the redeveloper as such parcel shall be deemed no longer in need of redevelopment. At the discretion of the Redevelopment Entity, the redeveloper may submit a final Certificate of Occupancy for approval by the redevelopment entity to serve as the Certificate of Completion.

RECOMMENDED HORTICULTURAL PRACTICES

APPENDIX 1.0 RECOMMENDED HORTICULTURAL PRACTICES

1.0 Soil Handling and Top Soil

- 1.1 Soil excavated from construction areas shall be used as under-slab compacted fill or removed from the site. Any contaminants found must be remediated.
- 1.2 All areas to be landscaped are to receive topsoil that is friable, fertile natural loam, free of subsoil, stones, roots, noxious plants and extraneous matter to a depth of 3 feet from finished grade as a subsoil cap and new planting root growth zone.
- 1.3 The zone of discontinuity between existing subsoil and top soil should be broken to provide a transition zone if permitted by environmental remediation agreements. Till the subsoil, add several inches of topsoil then till these together before adding the remaining topsoil.
- 1.4 Provide continuous 3-foot deep trenches of high quality topsoil for planting of street trees along streets to widths as specified between municipal curb and sidewalk in lieu of individual tree pits.
- 1.5 Soil depth on rooftop gardens can vary according to plant type. Minimum soil depth for large trees should be 36 inches deep or 6 inches deeper than the root ball; 30 inches deep for small trees, 24 inches deep for shrubs and 12" deep for lawns or groundcover.

2.0 Soil Compaction

- 2.1 The deep soil structure of planting areas within parks and street allowances shall be loosely compacted and protected by barriers during construction from heavy compaction by equipment and stockpiling of materials.
- 2.2 Granite cobbles or unit pavers set on an aggregate setting bed shall be placed around street trees where it is necessary to facilitate pedestrian movement directly at the base of each tree.
- 2.3 An expanded slate soil admixture can be added to the sub-base for modular paving over planting trenches between street trees to compensate for compaction and promote root growth.
- 2.4 Where heavy pedestrian circulation is expected, a concrete slab bridging over the continuous planting trench can provide a suitable pedestrian surface while preventing soil compaction in the tree trench. Another acceptable method to extend the root growth area is to use root pipes or drainage mats to provide root paths.
- 2.5 In areas where pedestrian circulation is not conflicting with continuous tree planting trench, provide vegetative ground cover at base of tree and protect planted trench at periphery with low ornamental fences.

3.0 Hydrology

- 3.1 All street tree plantings shall have surface watering/fertilizing access pipes and subsurface drainage outlets.
- 3.2 All sodded areas, planting beds and street tree strips shall have in ground irrigation systems.
- 3.3 Site grading and permeable surfaces shall promote maximum return of clean rainwater within parkland, with flat areas graded to 2% minimum. Contaminated surface drainage shall be carried away from landscaped areas.
- 3.4 All plants within rooftop gardens can experience a high evaporation rate due to the drying effects of wind and sun. Irrigation, mulches and moisture-holding soil additives shall be added to help reduce this moisture loss.

4.0 Plant Stock

- 4.1 Listed plant species shall be thoroughly searched by a plant broker before consideration of species not listed.
- 4.2 Street trees shall have a branch height of 6 feet minimum at planting.
- 4.3 All street trees shall be of 3.5 inches caliper minimum.
- 4.4 Red Oak shall be dug and planted only during the spring planting season.

5.0 Planting and Plant Care

- 5.1 Strategically phase street tree planting to ensure procurement of large quantities of uniform and consistently sized specimens of specifically selected species.
- 5.2 Plant trees only during appropriate Spring and Fall planting seasons to the highest arboricultural industry standards.
- 5.3 All trees shall be guaranteed against defects including death and unsatisfactory growth for a period of two years. All defective plants shall be removed immediately and replaced as soon as weather permits.
- 5.4 Fertilization shall be yearly with a balanced, full spectrum inorganic commercial fertilizer applied at a rate adjusted to remedy deficiencies identified by soil testing reports.
- 5.5 All trees shall be monitored and treated annually by the developer for potential disease or decline in physical condition.

DESIGN VOCABULARY

Design Vocabulary – Transit Plaza and Open Space



Transit Plaza and Open Space

Within the hardscaped transit plaza, varied paving patterns, water features and large plantings are recommended in order to create a pedestrian friendly environment. The inclusion of a multi-use trail within the redevelopment area will help promote alternate modes of travel as well as provide a needed connection to the currently marginalized stream.

Design Vocabulary – Elevated Sidewalk



Elevated Sidewalk

The recommended sidewalk mentioned in the redevelopment plan, should create places where people can congregate, while adding a strong landscaped element to the pedestrian realm.

Design Vocabulary – Building Facades



Building Facades

The ground floors of the buildings (facades below first stepback) in the redevelopment plan should be masonry construction providing a strong grounding for the structure, with retail shops incorporating awnings into their storefronts. The upper floors (facade above first stepback) should incorporate large sections of glazing in order to make the building seem light and airy.



Galleria

A key element of the redevelopment plan is the galleria. This design feature provides the access to the transit plaza and creates a unique break in the building facade at a major landmark corner



Parking Decks

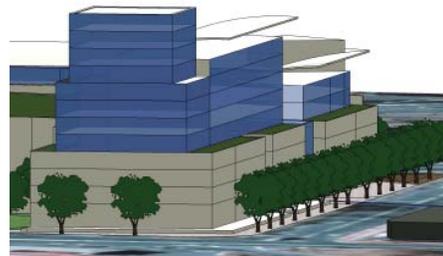
The design of the parking deck is a critical element to the success of this plan. As illustrated in these images the parking structure should not be a blank, exposed structure. The addition of an artificial facade should be adopted for the side facing the transit line. A liner building should be built for the sides of the building facing the street. While a green-faced facade utilizing vines is recommended for the side facing the trail.

Design Vocabulary – Three-Dimensional Illustrations



The models shown on this page illustrate several key components of the redevelopment plan.

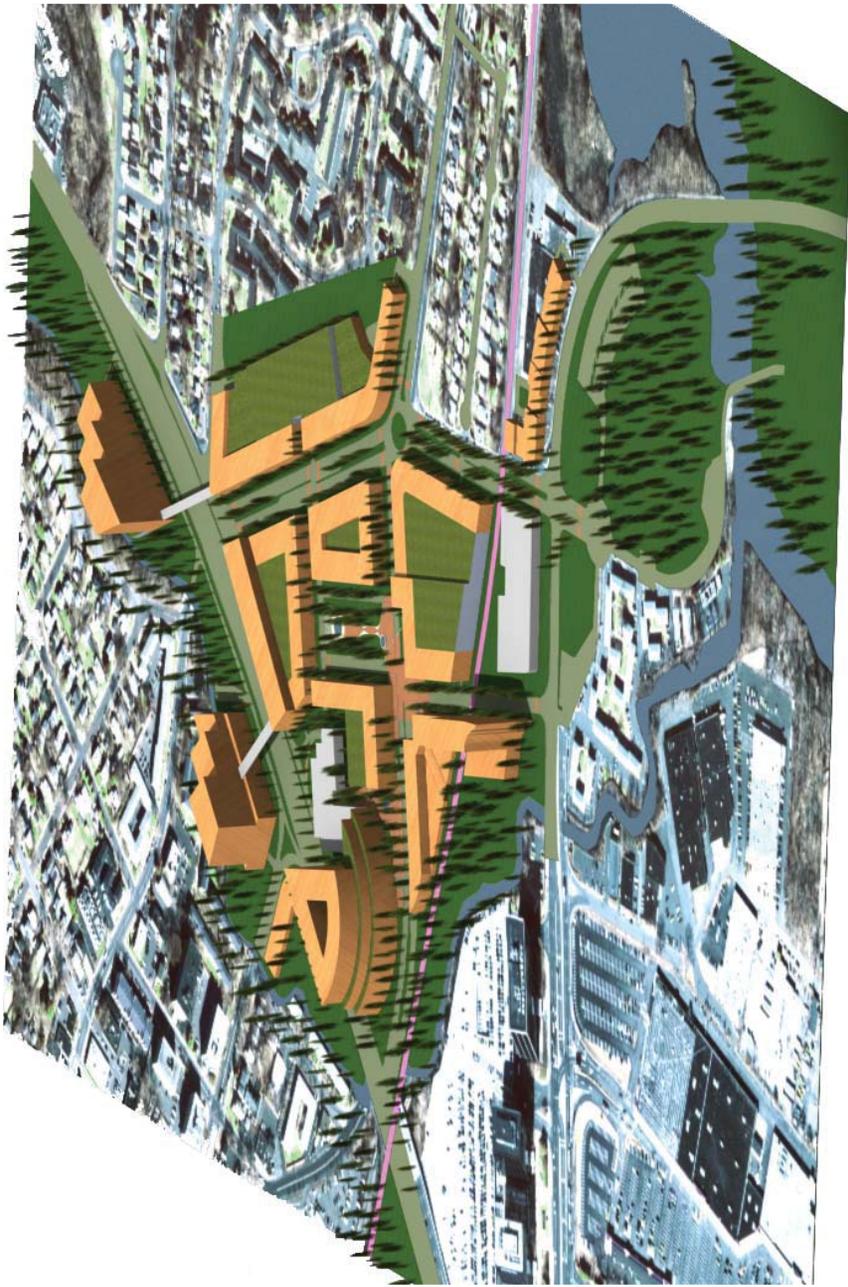
- The design of a strong base to the buildings that feels grounded to the pedestrian realm. While the upper portions of the buildings, designed heavily with glass, feel much softer and transparent.
- The landscaped transit plaza including seat walls, textured paving, trees, vegetation, and water features.
- A key tower that acts as a landmark along Kindekamack Road anchoring the mid-rise buildings adjacent to the site.
- The addition of numerous green roofs that aid in the cleaning and retention of the stormwater that falls on the site. As well as creating additional value for the units that overlook the parking structure.
- A green buffer along the stream edge that contains a multi-use pedestrian trail.



VISION PLAN: RECOMMENDATIONS FOR A NEW TRANSIT ORIENTED TOWN CENTER BASED ON PUBLIC INPUT

River Edge, New Jersey

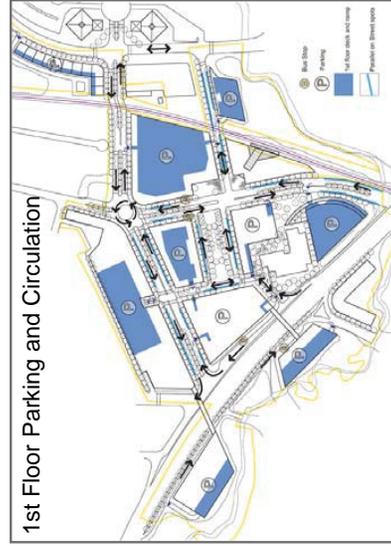
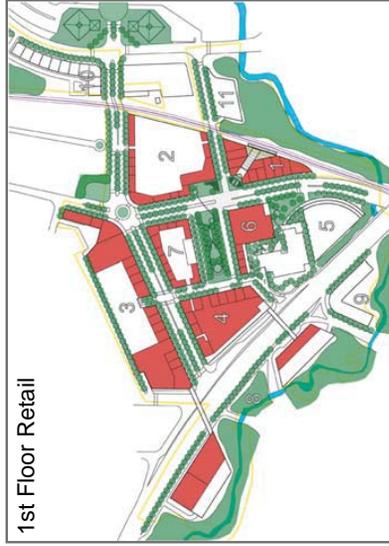
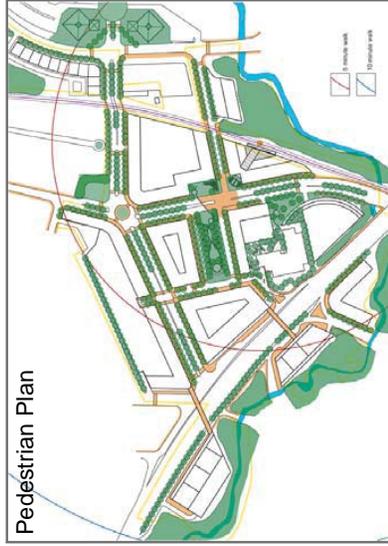
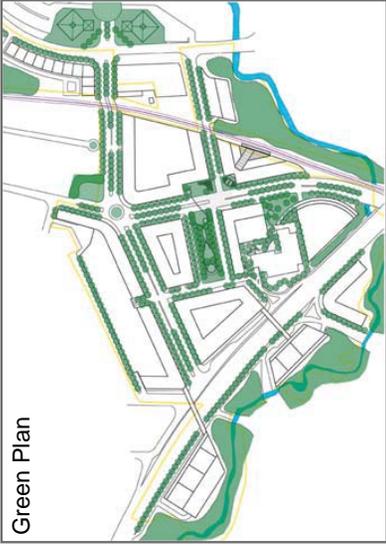
Recommendations For A New Transit Oriented Town
Center Based On Public Input



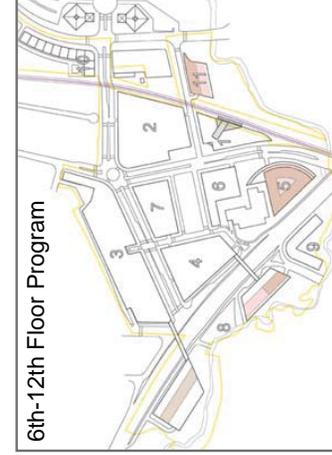
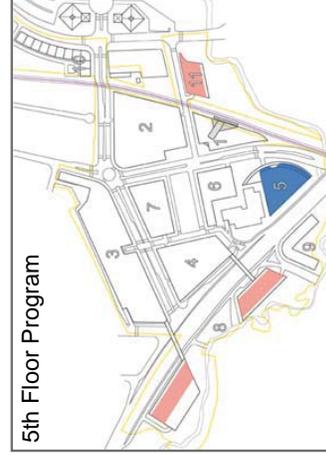
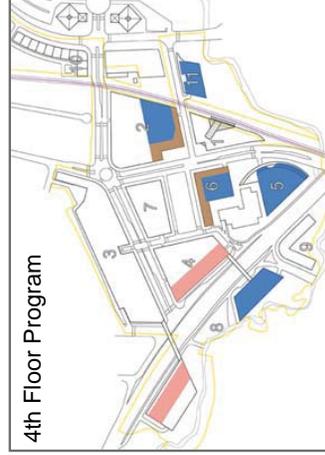
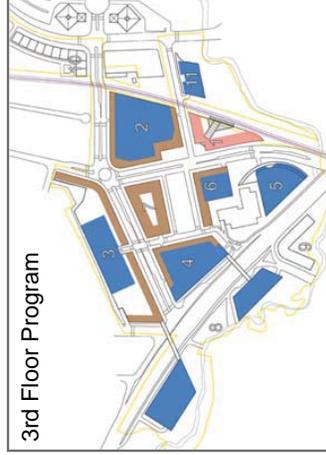
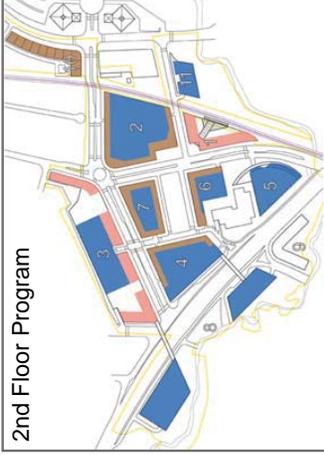
A. Nelesen Associates, Inc.
Visioning ♦ Planning ♦ Urban Design

August 1, 2005

Program Elements by Floor



- Retail Space
- Housing Units
- Office Space
- Parking Decks
- Sidewalks & Crosswalks



Proposed Conditions

Proposed Redevelopment															
PHASE/PARCEL	Retail		Office		Residential		Parking		Add'l parallel parking spaces		No. of Floors				
	Area, SF	Area, SF	Area, SF	Units	Area, SF	Units	Spaces provided in proposal	Required Parking**	NJT Parking	Future Parking	Retail	Office	Res.	Parking	Total
1 Station	37,515	58,030					0	0	306		1L	2		3	
2 Trackside	39,747	0	99,368	79	393,160		606	215	300	31	1L	3	4	4	
							1,023	820							
3 N of Main	108,499	56,857	46,214	37	192,120		549	537		41	1B	1	1	3	
4 W. of Square	83,912	60,198	18,103	14	168,602		482	477		22	1B	2	2L	2	5
5 South Tower	0	154,845	173,224	111	240,625		688	617		13	3	4	5	12	
6 S. of Square	39,057	0	49,641	40	74,613		213	169		29	1B	3L	3L	3	4
7 N. of square	22,337	0	70,668	57	52,789		151	128		32	1L	18	1L	2	3
8 S. of Rt.4	53,040	121,903	128,021	102	260,496		744	672		16	1L	varies	varies	3-4	12
9 Auto Dealer	26,644						80	80			1			1	
10 E. of Tracks		84,600	54,520	44	112,800		322	314		19		2	2	2	1
11 E. Tower							322	314		15		8	4	4	12
Total	410,751	536,432	702,471	509	1,495,205		4,302	3,840	300	246					

Total building area = 3,144,859 SF

Floor Notes:

- L = Liner outside of parking
- B = Includes "Big Box" Retail

*Future NJT parking allotment would be transferred to Parcel 2

**Parking estimates are based on typical ratios for transit-oriented development

1-10 space per 1,250 sq. ft. residential unit, or 2400 sq. ft. per townhouse

3-20 spaces per sq. ft. 1,000 for offices (except for parcel 3, which has 3 spaces per 1,000 sq. ft.)

3-20 spaces per 1,000 sq. ft. for retail

*** Total building area includes all residential, retail, office, and structured parking

maximum floors = 12

Dwelling Units/Acre	Proposed	Percent Growth
Net	17.5	
Gross	10.5	
Effect on Borough Revenue:		
Equalized Value (equal to Market Value)	\$ 573,734,495	692%
Assessed Value	\$ 334,142,970	
Property Taxes	\$ 12,029,147	
	3.60%	\$ 10,610,603

Suggested Redevelopment Area

