

Borough of River Edge, New Jersey

NEW BRIDGE LANDING STATION REDEVELOPMENT PLAN

The original of this Plan was signed and sealed
in accordance with N.J.S.A. 45:14-12

Prepared for:

The Borough of River Edge



Prepared: June 12, 2006

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INTRODUCTION

This Redevelopment Plan is prepared at the direction of the Borough of River Edge Mayor and Council in cooperation with the Borough of River Edge Planning Board.

A Redevelopment Plan is a work in progress. It is a guide which serves to provide a framework for land use and development within a redevelopment area; and also serves as an opportunity to encourage the residents, business owners and property owners to improve and renovate certain parcels of land individually or through public-private agreements, redeveloper agreements and other entities.

The designation of a redevelopment area and the adoption of this Plan provides an opportunity to provide much needed property tax relief as well as public and private improvements to encourage the highest and best use of this area and invigorate growth in the area.

This Redevelopment Plan serves as a tool to guide the revitalization of the New Bridge Landing Station Redevelopment Area. While the Borough Council may approve the initial Plan, the residents, businesses and property owners of River Edge must always have the opportunity to apply to modify and improve this Redevelopment Plan to achieve the expresses and desired goals. A good plan will be able to remain relevant with the times. That is possible only with the consistent input of the people of the Borough of River Edge. It is also critical to the successful implementation of this Plan to reach out to our community commercial property owners, with the sensitive and judicious use of redevelopment tools at the Borough's disposal.

It is also the policy of the Borough of River Edge that property owners and businesses located within the Redevelopment Area will be given every opportunity to participate in the redevelopment program through redeveloping their properties in accordance with the land uses, building and design requirements of this Plan.

DESCRIPTION OF THE PLAN

SECTION 1.0 DESCRIPTION OF THE PLAN

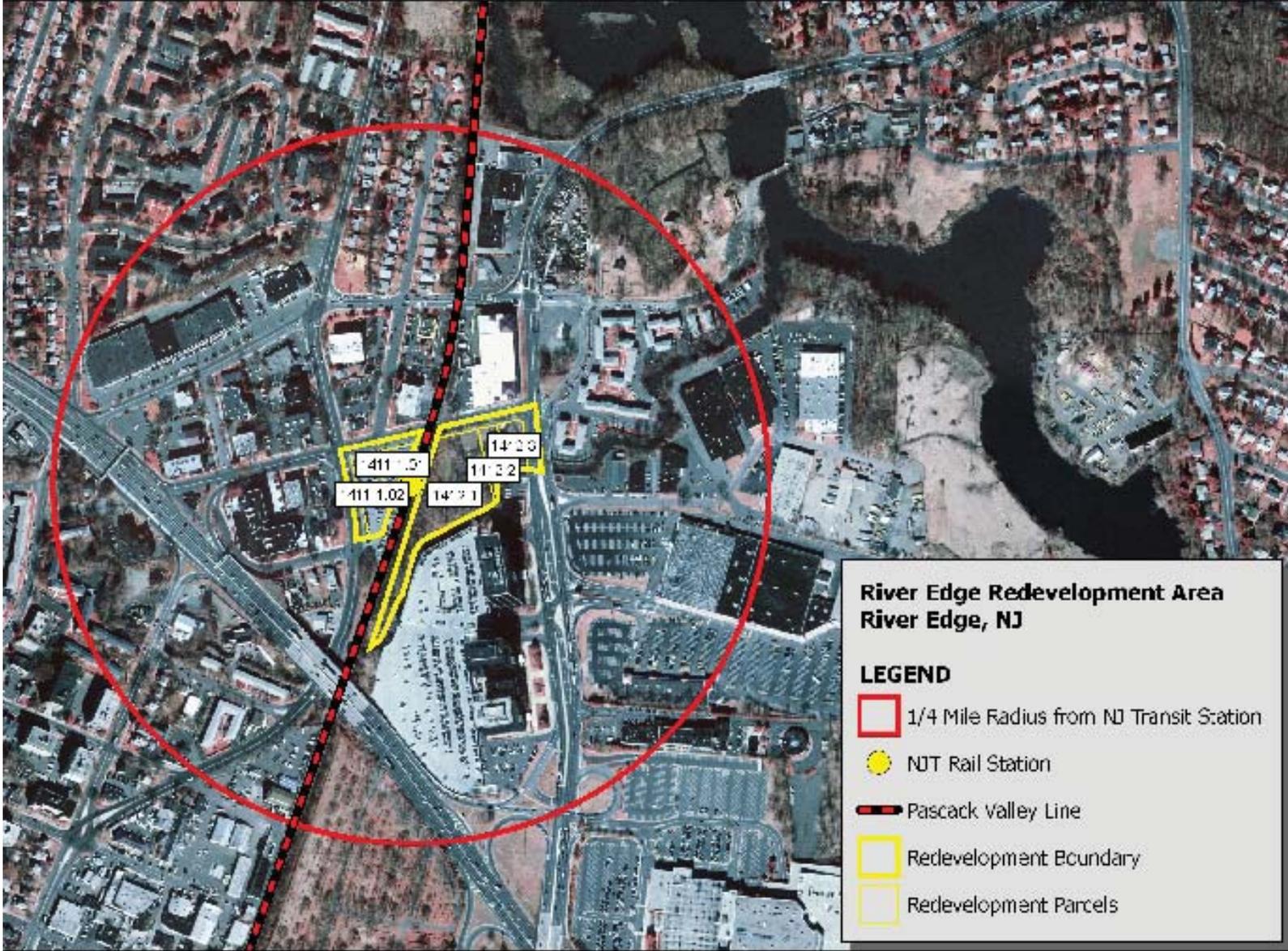
The New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area is comprised of approximately 3.8 acres net (excluding all rights of way). The gross area for the New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area including ROW's is 4.94 acres. The Redevelopment Boundary of the site is shown on **Exhibit 1.0, New Bridge Landing Station - Aerial Photo** and **Exhibit 2.0, Lot and Block Map**. The Redevelopment Parcels comprising the 3.8 acres are shown in the lighter yellow lines.

The study area consists of the following tax blocks and lots:

Block 1411 (Lots 1.01, 1.02)

Block 1412 (Lots 1, 2, and 3)

These parcels shall be reorganized into two redevelopment parcels, hereafter labeled A and B on **Exhibit 3.0, Development Parcels**. This exhibit shows the shape and location of each redevelopment parcel, which shall be hereafter referred to as parcels: A, that is the area on the southwest corner of Grand Ave and Kinderkamack Rd; and B, on the southeast corner of Grand Ave and Hackensack Ave. The New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area will hereafter be referred to as the "Area" or the New Bridge Landing Station Redevelopment Area.



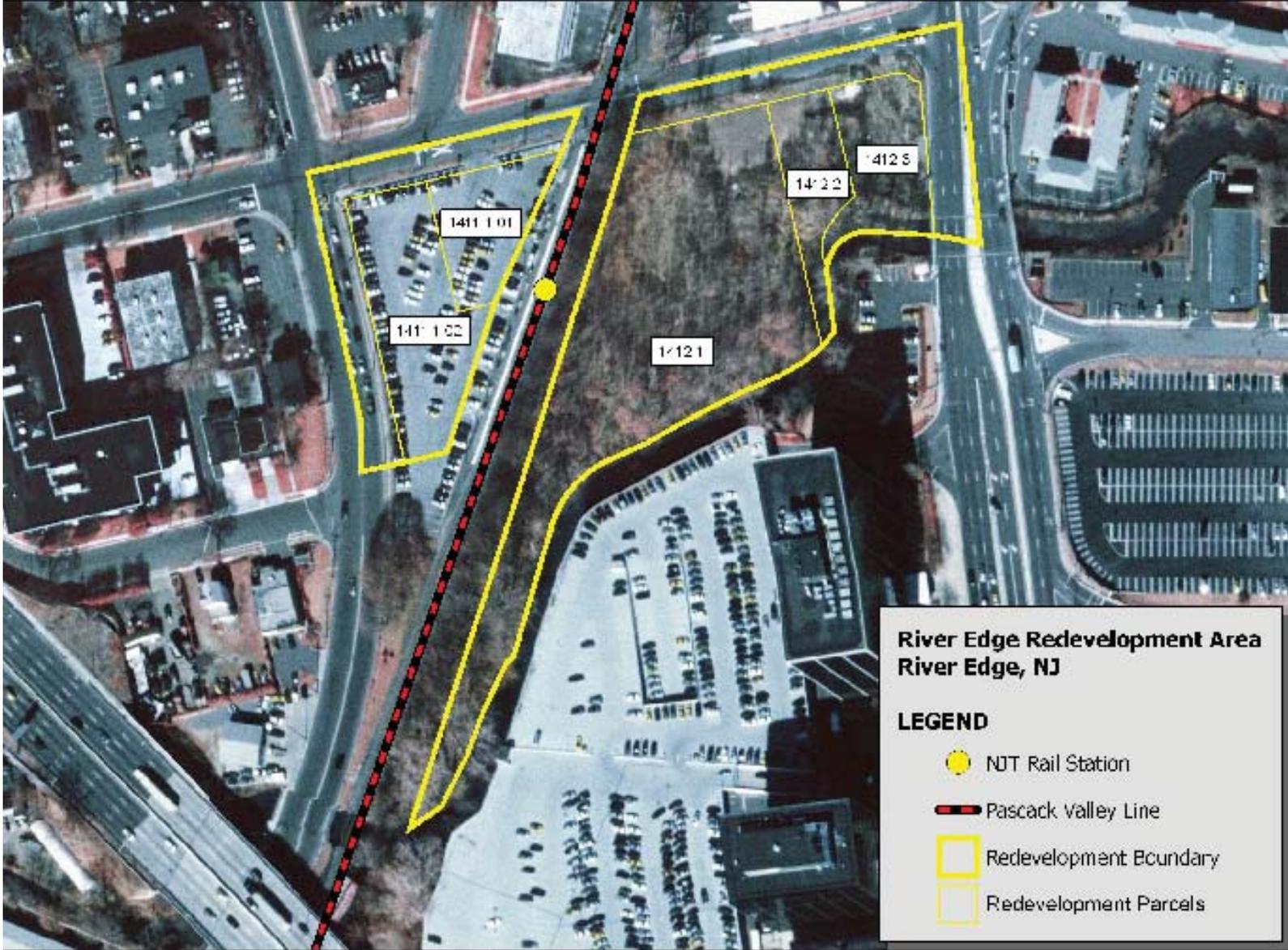
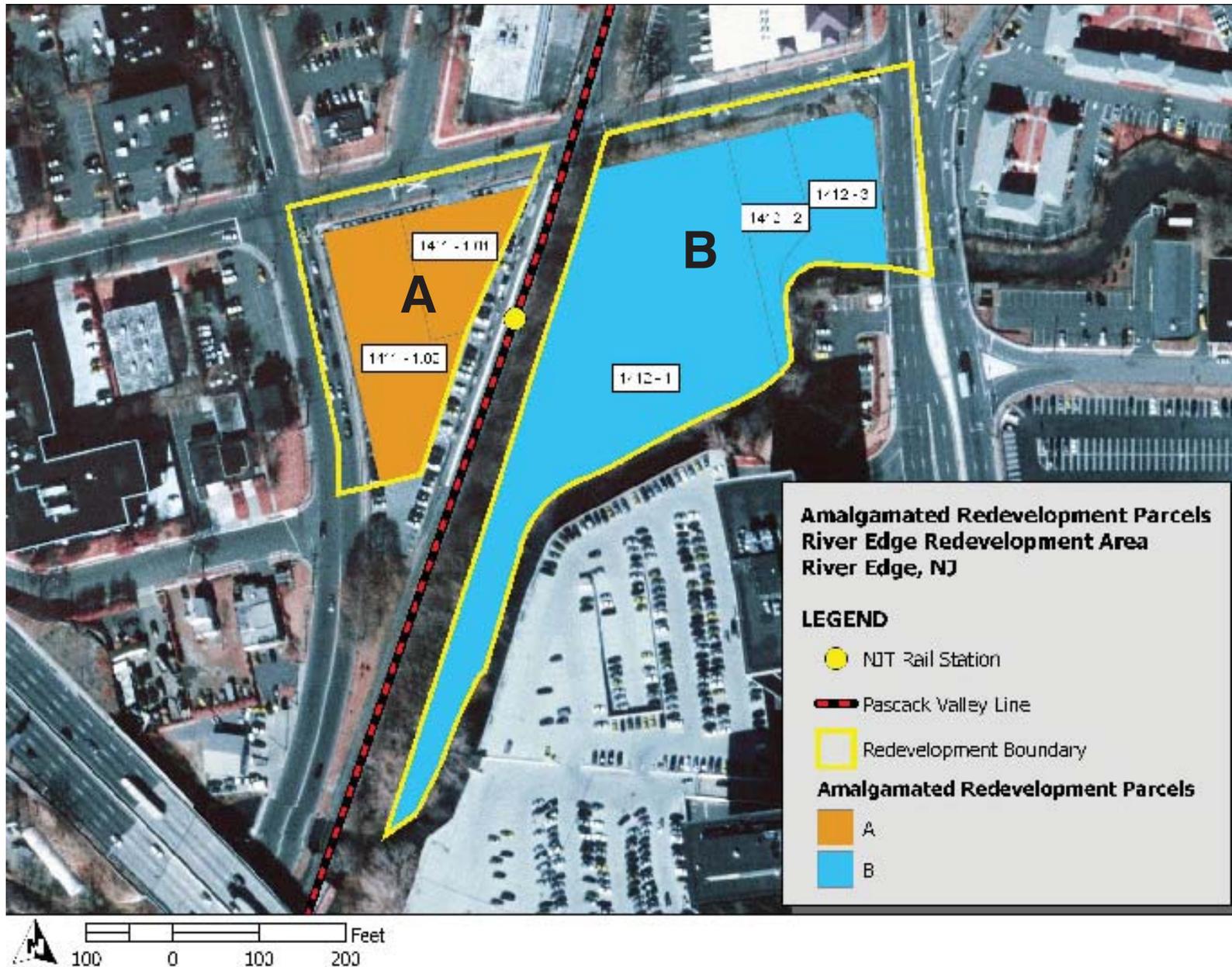


Exhibit 3.0 Amalgamated Redevelopment Parcels



STATUTORY BASIS OF THE PLAN

SECTION 2.0 STATUTORY BASIS OF THE PLAN

The Plan has been prepared in furtherance of the Borough of River Edge Council Resolution #05-123 determination on March 7, 2005, that the area meets the statutory criteria for designating an “area in need of redevelopment” pursuant to the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et seq. (the “LRHL”). The Borough of River Edge Council subsequently directed its consulting planners, A. Nelessen Associates, Inc., to prepare a Redevelopment Plan. The consultants submitted their plan, entitled *New Bridge Landing Station Redevelopment Plan*, to the Borough of River Edge in June, 2006. For this Plan to become effective, it must be adopted by the Mayor & Council by Ordinance pursuant to N.J.S.A. 40A:12A-7.

PURPOSE OF THE PLAN

SECTION 3.0 PURPOSE OF THE PLAN

The purpose of this Plan is to set forth the terms and conditions under which the New Bridge Landing Station (North Hackensack Rail Station) Redevelopment Area may be redeveloped by a designated Redeveloper(s). The Plan describes, among other regulations, the basic townscape requirements and recommendations. The Building Regulating Plan includes the figure-ground plan, general massing, build-to lines, and height limitations of buildings. The Architectural Standards describe the character of the buildings. The Landscape Plan includes the landscape and streetscape requirements. The Design Vocabulary illustrates the suggested building style, character and streetscapes.

The physical components of the plan are both written and illustrated using a form-base type code. This type of code is the most appropriate to assure the highest visual and spatial quality of town design. It assures that the redevelopment entity can expect a redevelopment of superior quality and character that will complement existing character of the town be reflective of the Vision Plan prepared for this area (attached in **Appendix 3.0**) and have a long term positive fiscal, aesthetic, and emotional impact on River Edge as the area transitions into the Town Center of River Edge. It further assures that the redeveloper can build and market a superior product that is financially feasible and finally that there is some architectural design flexibility.

GOALS OF THE PLAN

SECTION 4.0 GOALS OF THE PLAN

By adoption of this New Bridge Landing Station Redevelopment Plan, the Borough of River Edge seeks to accomplish the following goals:

- A. To eliminate underutilization of the designated Area and to eliminate blighting influences.
- B. To maximize the advantages provided by the New Bridge Landing Train Station and other mass transportation resources thereby becoming a transit oriented development.
- C. To allow for more efficient use of land and to expand the Borough's tax base by encouraging high quality mixed-use development.
- D. To build sufficient parking to meet the needs of the transit commuter, the businesses and residents of this Area.
- E. To expand the level of residential and commercial activity in the Area, thereby increasing the potential for economic activity and job creation.
- G. To maximize the participation of private developer(s) while minimizing the participation of the public sector.
- H. To create a well planned and designed development Area which will provide opportunities for mixed-use, retail, multi-family residential, entertainment, recreation, permanent employment, and commercial facilities within an area that is currently underutilized but has the potential for sound development that will improve quality-of-life and the visual and spatial character of River Edge.
- I. To provide a system of sidewalks, crosswalks streetscapes, landscapes and building wall treatments at the ground level that encourage a safe, engaging and pedestrian-friendly experience that will enhance walkability.
- M. To provide for the creation of places and pedestrian realms which promote pedestrian activities, social interaction and citizen security.
- N. To enhance the use, visual character, safety, and functionality of the New Bridge Landing Station.
- O. To provide market-value housing and commercial and retail development through new construction of mixed-use buildings.
- P. To encourage more people to take up permanent residence within transit-oriented development and to walk to transit and adjacent uses.
- Q. Design buildings for modern, mixed-uses.
- R. To create mixed use buildings, parks, plazas, streetscapes and pedestrian realms that will act as a focus area for the Borough as a transit center.
- S. To begin the first phases of developing a real "Main Street" and town center for the Borough of River Edge.
- T. To utilize Coles Brook's edge as an amenity in the Plan
- U. To create opportunities for shared parking thereby not requiring all lots to accommodate their individual parking needs.
- V. To provide high quality building design
- W. To maximize energy savings through the use of green building methods, compact design, and walkability.
- X. To maximize the adjacency to New Bridge Landing park
- Y. To preserve the existing supply of commuter parking at NJ Transit's North Hackensack Rail Station and provide for future demand.
- Z. To encourage appropriate economic development while minimizing its costs on the public resources and facilities of the Borough of River Edge.

The goals set forth are intended to be broad, general policy statements. While it is possible that specific aspects of the Plan may result in minor variations or deviations from these goals, it is intended that the overall result of the implementation of this Plan will be consistent with the goals as set forth above.

OVERVIEW OF THE PLAN

SECTION 5.0 OVERVIEW OF THE PLAN

5.1 THE ILLUSTRATIVE SITE PLAN

The Area that is the subject of this Plan presents a unique opportunity for the Borough of River Edge. The site is ideally located. It is currently surface parking for the transit stop, and vacant, wooded land. This is an ideal opportunity to maximize the benefits of transit by transforming this underutilized land into a transit-oriented development. The Area has the potential for a level and quality of development that will transform the area, redefining the station area from an open parking lot to an area that will be a major contributor to a highly desirable pedestrian oriented mixed-use classic, yet modern, town center.

The plan focuses redevelopment around the NJTransit stop while promoting pedestrian connections to this important resource. The Plan focuses on building a townscape appropriate to the location while adopting the standards of Smart Growth, Transit Villages, walkability, and safety through “eyes on the street.” from residential units and active transparent store fronts with quality streetscapes.

The site is a challenge and an opportunity for innovative design. The site has a particular shape. Portions of the site lie within the 100 year floodplain. Part of the site is used for parking. One edge has a dirty stream, and it is adjacent to undistinguished tall office towers. The opportunities are its location immediately adjacent to the existing train stop; it is the first phase of the development of a new transit oriented town center for the Borough and its proximity to the remainder of the Area in Need of Redevelopment and has the ability to be phased.

The Plan meets these challenges and focuses on the creation of great pedestrian friendly streets by sitting buildings at street edges; enhancing the pedestrian realm by wide tree lined sidewalks and crosswalks; creating a new transit plaza; screening the shared parking by using liner buildings; enhancing sidewalk activity through ground floor retail; creating a very desirable place to shop, work, live and plan through building and street design; providing unique architectural facade treatment which emphasizes the 3-6 story base of the buildings..

The Plan is intended to create well proportioned and human scaled buildings and street spaces through the use of build-to-lines, setbacks and stepbacks, apparent building sized, bay spacing, roofs and cornice treatment, landmarking of corners, street proportions, and a pallet of materials and complementary landscaping.

In order to demonstrate how the Area could be structured so as to optimize its potential, a concept Illustrative Site Plan for the Area is presented in Exhibit 4. This Illustrative Plan shows the optimum configuration of the mixed-use buildings, the location and character of the pedestrian realm, the park, plazas and other openspaces, and the recommended locations of ingress and egress from the parking.

As can be seen on **Exhibit 4.0**, it is the intention of the plan that flat roofs and terraces be “green”, i.e. covered with landscaping and accessible, thereby enhancing the ecological appeal, decreasing runoff, lowering heat and energy costs and reducing urban heat island effect.

The New Bridge Landing Station Redevelopment can fulfill the role of capitalizing on underutilized land while promoting town design that provides a pedestrian friendly environment necessary for high quality mixed-use residential/commercial buildings and parking while bringing new street life to this area. When developed, the Plan will add market-rate housing, the required proportion of affordable housing, expand the range of retail, office, and possible live/work offerings, and provide a catalyst for future redevelopment within adjacent areas.

The Illustrative Plan is an example of how this Redevelopment Plan could be implemented. It is a valuable tool to visualize the location of buildings, the streetscape, the pedestrian realm and open space. Actual building form may vary as to its particulars, but the key elements of the Illustrative Plan, such as the building locations, new street network, arcades, enhanced pedestrian realms, the land devoted to open spaces and plazas and the careful massing of structures to provide both architectural interest and appropriate levels of light and air must substantially conform to this ground plan. Small deviations are expected and allowed.

5.2 PROPOSED REDEVELOPMENT ACTIONS

In carrying out the Plan, the Borough of River Edge and the designated redeveloper intends to undertake a variety of redevelopment actions. These will include but not be limited to:

- A. Clearance of all dilapidated and under-utilized structures, surfaces and other debris.

SECTION 5.0 OVERVIEW OF THE PLAN

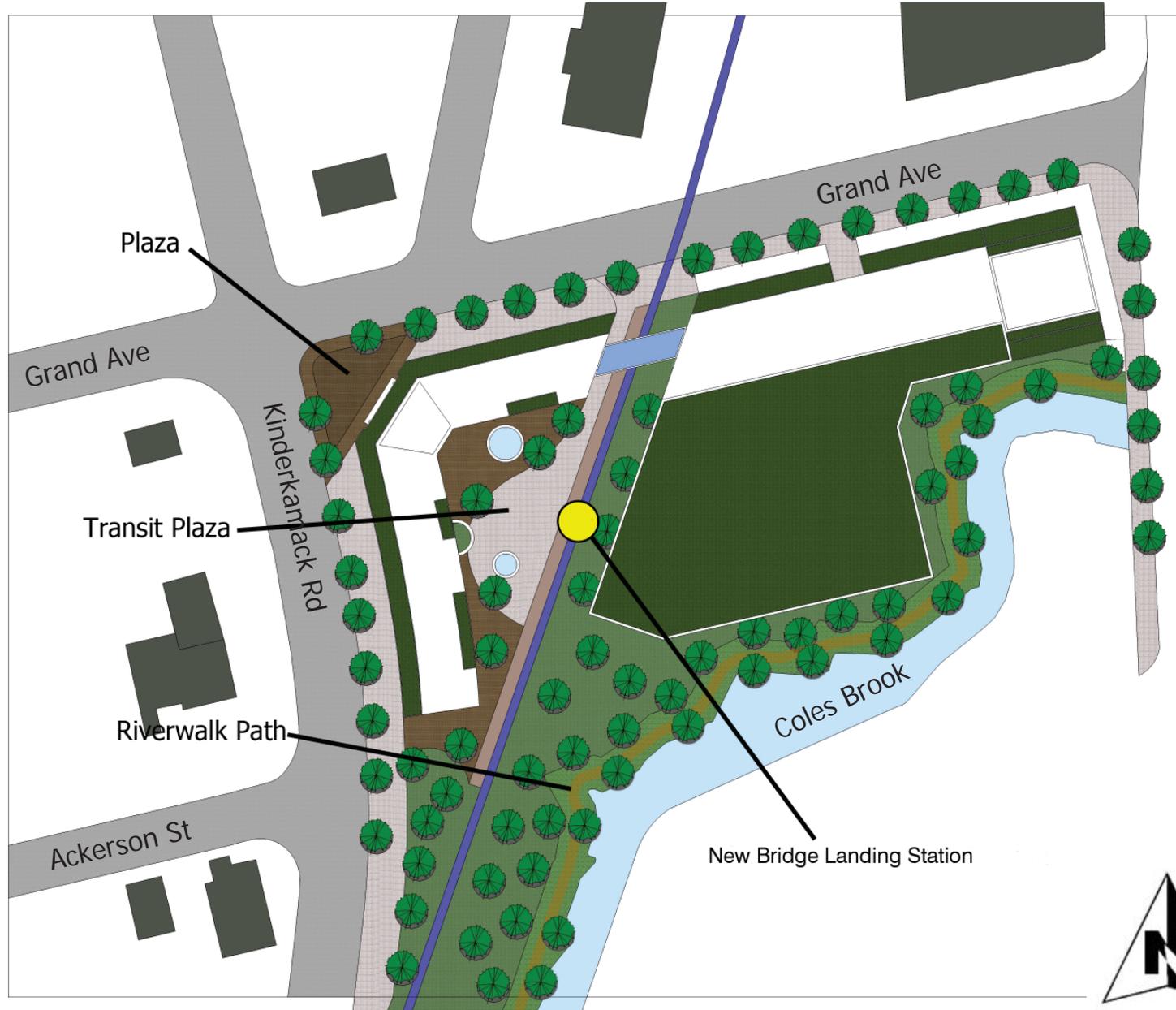
B. Assembly of suitable parcels of publically owned land for the construction of the proposed uses set forth in this Redevelopment Plan. These uses may include: retail, residential, offices, hotel, structured parking, mixed use, live-work units, plazas, parks and pedestrian walkways.

C. Improvement of streetscapes, crosswalks, pedestrian amenities and the entire pedestrian realm.

D. Construction of private buildings and other improvements appropriate to the purpose of this Plan.

E. Improvement, revitalization and beautification of the Area.

F. Construction of the “first phase” of a transit oriented, town center and Main Street for the Borough.



DEFINITIONS

SECTION 6.0 DEFINITIONS

Terms in this Redevelopment Plan shall be defined as indicated:

Area – Shall mean the New Bridge Landing Redevelopment Area

Bay – The distance between structural elements of a building that is reflected on the facade of a building.

Borough – The Borough of River Edge, NJ.

Build-to-line – A line along which the primary facades of a building must be located. The build-to-line allows flexibility to the articulation of the facade, allowing the facade to deviate in limited increments from this line.

Building Height – For the purpose of this Redevelopment Plan, the height of the buildings will be expressed in number of stories, with a range of floor to floor heights, counting up from the adjacent ground plane or sidewalk.

Context – The particular combination of elements that create specific habitat. Context includes building use, density, height and setback, and other elements of the intended habitat, including those of the private lot and building as well as those of the enfronting public streetscape.

Corridor-Urban – A linear geographic area incorporating buildings typically on both sides of the street including the pedestrian realm all in proper proportions to create a sense of street space.

Curb – The edge of the vehicular pavement detailed as a raised masonry step. The Curb usually incorporates the street drainage inlets.

Density – The number of dwelling units within a standard measure of land area, usually given as units per acre.

Developable Area – The developable area is the allowable building footprint of the ground floor of the building. It is within this designated area on the specific parcel that the building can be located.

Design Speed – The velocity at which a thoroughfare tends to be driven without the constraints of sign age or enforcement. There are three ranges of speed: Very Low (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High (above 35 MPH). Lane width is determined by desired design speed.

Elevation (drawing) – The exterior walls of a building. An elevation drawing includes material, rendered window and door openings, height and facade details. See Facade.

Encroachment – An area beyond the build-to-line that certain building element can protrude. The encroachment distance is typically expressed in feet. Typical encroachments may include overhangs, bow and bay windows, signage, porticos, steps and stoops or other elements that commonly protrude over the main facade of a building.

Façade – Any vertical, exterior face or wall of a building.

Green Roof - a roof of a building that is partially or completely covered with plants that can be used to retain stormwater, as well as for recreation (both passive and active). See **Section 10.1.7** for further detail and specifications.

Identifiable Building Widths – A subdivision of a linear facade to look like a series of adjoined buildings.

Live-Work – A dwelling unit that contains a commercial component. The commercial component can be located anywhere within the unit but is typically located on the ground floor connected internally with a stair to a residential unit. The “work” portion of the live-work unit must have a separate entrance from the “live” portion. The two portions can be adjoined by a connecting door or staircase that is internal to the live-work unit.

Mandatory Standards – Phrases or sentences that contain the following words: require, must, and shall

Manufacturing operations – Any business that engages in the making of goods or wares by manual labor or by machinery.

SECTION 6.0 DEFINITIONS

Plan – Shall mean the New Bridge Landing Redevelopment Plan.

Public Realm – Shall mean the areas that are both used and seen by a person walking.

Redevelopment Entity – Shall mean the Borough of River Edge Council or designated entity.

ROW – Right Of Way

Sidewalk – The paved layer of the public frontage dedicated exclusively to pedestrian activity.

Semi-Public space – The yard area in front of a residential unit defined by a low fence and/or gate through which a person must pass in order to gain access to the front primary entrance. Semi-public spaces are also typically located in front of smaller offices.

Setback – The required number of feet that a building must be setback from a specified line, be that a property line, right-of-way line, stepback line or curb line.

Stepback – The location where the building must be stepped back from the lower facade plane.

Street – A throughway that has emphasis on both vehicular and pedestrian movements

Streetwall – The elevations of buildings that when seen from the street or sidewalks form the space container.

Suggestive Standards – Sentences or phrases that include the following words: recommended, may, suggests

Yard, Front – An open and unoccupied (except for driveways) space, unless occupied by a use as hereinafter specifically allowed, extending across the full width of the lot and lying between the front street property line and the

nearest line of the building.

Yard, Side – An open and unoccupied space, unless occupied by a use as hereinafter specifically allowed, on the same lot with the building between the building and the side lot line, extending from the front yard to the rear yard.

Yard, Rear – A space unoccupied except by an ancillary building structure or use as hereinafter specifically allowed, extending across the full width of the lot between the rear line of any building, other than an ancillary building, and the rear lot line.

MOBILITY PLAN

7.1 THE CONCEPT DESIGN

Streets are the Borough’s most important public spaces. Streets function as the circulation for both vehicles and pedestrians. Streets must not only be functional but also beautiful and safe. It is from these streets that the perceived character of the town center will be determined. Streets include not only the cartway, curb and parking edge, but also the landscaping, streetscaping, sidewalks, arcades, signing and building or street wall. Streets play a key factor in market “curb” appeal.

The functional, aesthetic and perceptual characteristics of streets must be positively optimized and understood as a component of townscape, resulting in places with the highest value and quality of life. It is the design of the streets, streetwall, street network, vistas and landmarks that allow easy, legible and understandable movement of both vehicles and pedestrians throughout the site and the town.

The Mobility Plan includes a Parking Plan and a Pedestrian Circulation Plan.

7.2 PEDESTRIAN CIRCULATION PLAN

The signs of a successful townscape are large numbers of people walking on the sidewalks. People will walk if there are pleasant and engaging places to walk and destinations on both ends of the walk.

The Pedestrian Circulation Plan must be designed to encourage walkability along the street edges, and across streets, with safe pedestrian, crossing of the rail at grade as well as via skyway connecting the parking to the train station. Walking must be safe a pleasant to and from - retail shops and offices, the train station, institutional and civic places, adjacent neighborhoods, and along the proposed Riverwalk. This is a transit-oriented development, putting an even greater reliance on people walking.

The Pedestrian Circulation Plan is a key component of Transit-Oriented Development by providing a safe and pleasant walking experience to and from the NJT rail station. Pedestrianism is dependent on the adequacy of sidewalk width, quality, safety and visual interest of the walking experience. There must be a continuity of the pedestrian experience including crosswalks that maintain sidewalk textures/treatments across the streets while providing appropriate lighting, and the landscape treatment of the entire pedestrian realm. Because the peak pedestrian travel times are expected to be morning and evening work trips, during lunch, on weekend days and during evening dining hours, lighting and retail availability along all sidewalks and the River walk must be coordinated.

The Pedestrian Circulation Plan, illustrated in **Exhibit 5.0**, is a continuous network of sidewalks, paths, crosswalks and plazas designed to provide a positive walking experience that focuses on the train station.

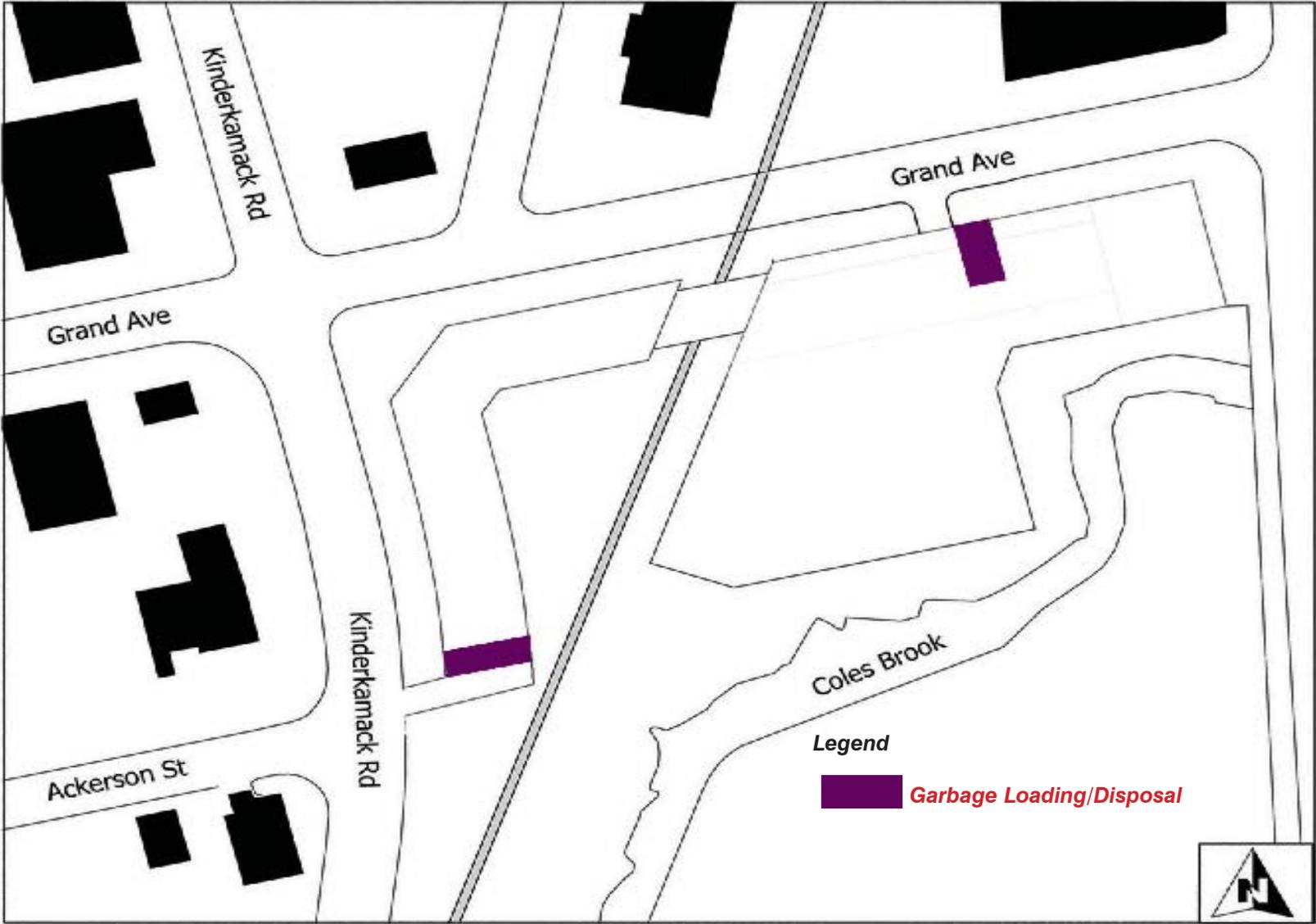
The required pedestrian circulation elements are as follows:

- A. Sidewalks shall be continuous along each block face that has street frontage.
- B. Sidewalks must be a minimum of 15 feet wide containing sufficient area for tree planting and decorative lighting and other street furniture like trash baskets, bicycle racks and benches. In specified locations and providing they meet specified standards, outdoor cafes are allowed.
- C. Proposed lighting standards and sidewalk materials shall match those currently installed on Kinderkamack Road.
- D. Crosswalks: Pedestrian crosswalks are required at locations specified in the Pedestrian Circulation Plan. This should be a very wide crosswalk, minimum of 15 feet and if possible raised 4 to 6 inches above the grade of the street to promote pedestrian safety. The interior of the intersection formed by the crosswalks shall be textured with cobblestones or other rough paving material that will encourage traffic calming. Pedestrian activated traffic light is highly recommended.
- E. Skyway over railroad connecting parking to the station will be a minimum of 25 feet above top of rail (per NJ Transit standards).

7.3 LOADING AND DISPOSAL REQUIREMENTS

- A. Each building shall be designed so as to accommodate easy access to garbage dumpsters and recyclable material containers by residents, tenants and carters. Enclosed garbage and recycling facilities shall be provided within each building and be sized for expected users. Recommended locations are shown on **Exhibit 6.0**.
- B. All garbage and recycling shall be screened from view from the exterior of the building.





C. Goods and services: package delivery for ground level retail and office can be across the sidewalk provided that a loading zone is designated on the curb edge.

7.4 PARKING REQUIREMENTS

| Use | Parking Requirement | |
|-------------|-------------------------------------|--------------------------------------|
| | minimum | maximum |
| Residential | 1.25 spaces/unit | 1.5 spaces/unit |
| Retail | 3.0 spaces per 1,000 sf | 4.0 spaces per 1,000 sf |
| Office | 3.0 spaces per 1,000 sf | 4.0 spaces per 1,000 sf |
| Hotel | 0.75 spaces per room | 1.25 spaces per room |
| Restaurant | 1.0 space per 4 seated table spaces | 2.0 spaces per 4 seated table spaces |

The minimum and maximum number of spaces for commercial (retail or office) and residential is provided in the chart above. Parking shall be provided at a minimum number of spaces and cannot exceed the maximum. The parking spaces per unit for any provided residential unit in any of the proposed buildings, is specified regardless of number of bedrooms. Parking for any unit or commercial uses that desire additional parking spaces shall be provided in ancillary parking or shall take advantage of any shared parking opportunities that will occur in the parking facility. This has an additional advantage of further activating the streetscape, improving street safety, and freeing land to be available for a transit station plaza. The number of spaces allocated for commercial uses shall be based on the gross square footage of the commercial space. Any on-street parking parallel on the redeveloped side of the street in the Area can count to meet the minimum need for commercial uses.

The maximum number of parking spaces has been specified because this redevelopment is transit-oriented, with walkable destinations and transportation, and is expected to attract a high percentage of empty nester older households that statistically have fewer cars.

Shared Parking: In order to promote more efficient use of parking facilities, a parking space may be counted towards the parking requirement for two or more different uses, provided that:

1. The applicant exhibits to the Planning Board’s satisfaction that demand for these shared parking spaces by each use, based on time of day, will not significantly overlap; and
2. No more than 50 percent of the parking spaces counted toward any use are shared spaces.

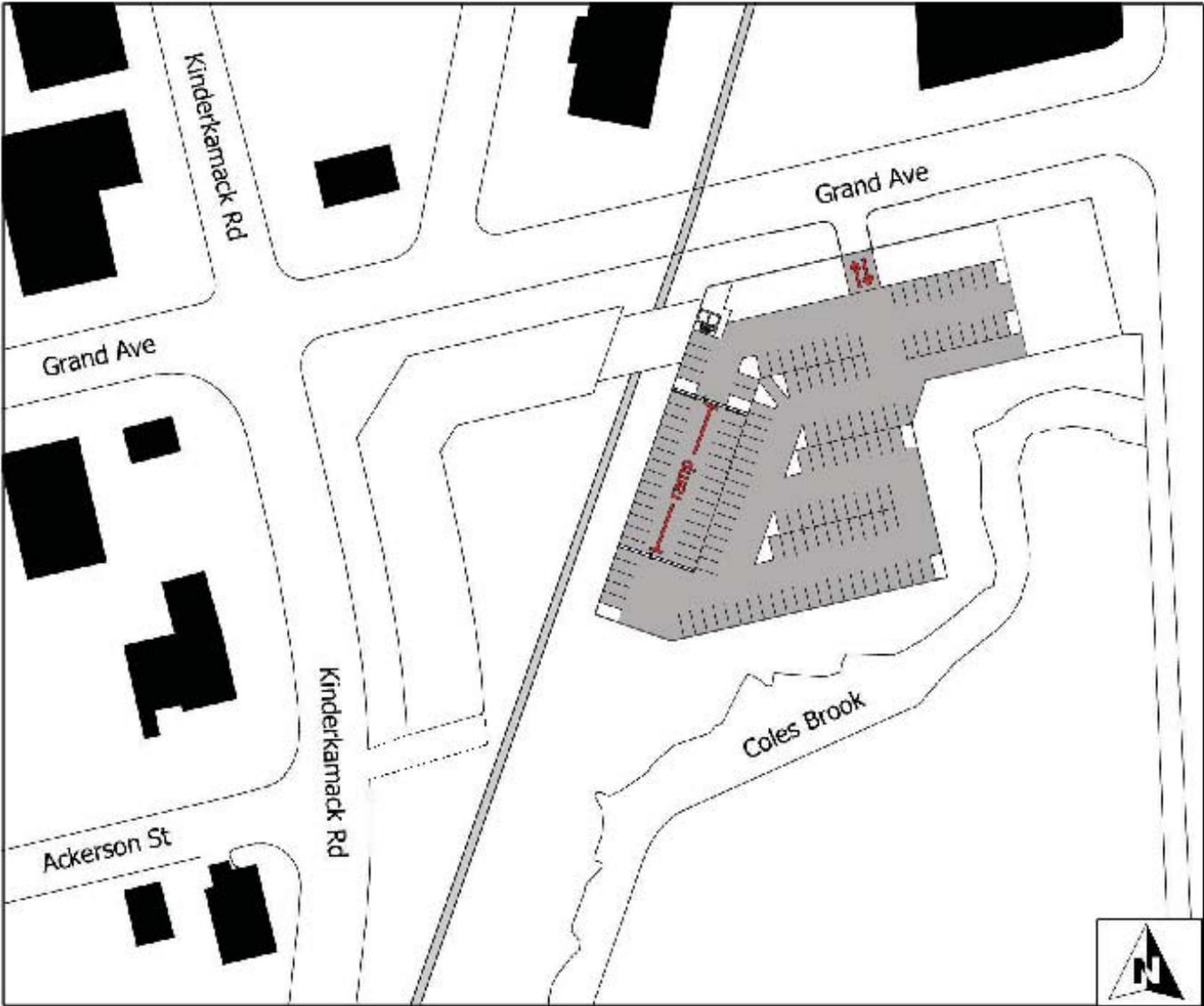
A potential developer must perform a traffic study and parking plan which must be approved by the Planning Board before development begins.

7.5 ADDITIONAL PARKING REQUIREMENTS

A minimum of 500 to a maximum of 550 parking spaces shall be provided for New Jersey Transit for use by commuters.

7.6 PARKING PLAN

Exhibit 7.0 illustrates a potential configuration of the parking structure on Redevelopment Parcel B. The travel lanes of the entire structure are two-way. This configuration supplies approximately 150 spaces per level. See **Exhibit 14.2 and 14.3** for a cross-section and the building regulations for this structure.



LAND USE PLAN

8.1 GENERAL REQUIREMENTS

In the event of any conflict or inconsistency between the provisions of this Plan and the provisions of The Borough of River Edge's Land Use Regulations, this Plan governs. The continued use of existing properties made non-conforming by adoption of this Plan is permitted until the property is to be redeveloped or substantially rehabilitated, at which time the provisions of this Plan shall apply. In the case where a particular land use or site standard is not covered in this Plan, compliance with the Borough of River Edge's Land Use Regulations or other applicable codes or Ordinances will be required.

Specific application of the land use and development requirements of this Plan, as they affect existing uses will be as follows:

- A. Existing uses that are non-conforming with the current zoning provisions will remain non-conforming unless they are expressly permitted in this Redevelopment Plan.
- B. Existing principal or accessory uses of properties, permitted by the use provisions of the Land Use Regulations in effect for the Area immediately prior to the effective date of this Plan, but which are not listed as permitted uses in this Plan, will become prior non-conforming uses at the time this Plan is effective. At that time and thereafter, any modification or expansion of these prior non-conforming uses is prohibited and any new use, redevelopment or rehabilitation of such properties shall be subject to the provisions of this Plan.

8.1.1 Exceptions to the General Requirements

Variation(s) from the development requirements and design standards set forth by the Plan may be necessary in certain limited circumstances. In such circumstances, the Planning Board may grant reasonable exceptions from certain bulk or design requirements if the designated redeveloper demonstrates that such design exceptions will not substantially impair the intent of the redevelopment plan, and will not present a substantial detriment to the public health, safety, and welfare.

To gain approval of such modification or waiver of a development requirement or design standard, the applicant shall demonstrate that the resulting change will:

- A. Generally satisfy the Redevelopment Plan's goals and purposes;
- B. Be designed in accordance with the Borough of River Edge's normal acceptable engineering, planning and/or architectural practices;
- C. Not have any adverse impact on the physical, visual, or spatial characteristics of the overall development plan for the parcel or tract to be developed, or adjacent or nearby properties;
- D. Generally enhance the overall development plan for the tract;
- E. Not have an adverse impact on the physical, visual or spatial characteristics of the existing streetscape in which such development is located or of the Redevelopment Plan;
- F. Not reduce the useful life or increase the costs of maintenance of the improvement to be modified or otherwise have an adverse impact on the long-term function of the development; and

Deviations from the uses permitted in the Area shall be permitted only by means of an amendment of the Redevelopment Plan by the Borough of River Edge, should the deviation be considered acceptable to the furtherance of the goals and purposes of this Plan.

8.1.2 Provisions Related to Rehabilitation

Rehabilitation and redevelopment of all buildings and improvements in the redevelopment area shall be made by property owners and/or developers in accordance with the standards included in this Plan as well as all other applicable Borough of River Edge and State codes, regulations and standards.

8.1.3 Provisions Related to Off-Site Improvements

The extent of the Redeveloper's responsibility for any installation or upgrade of infrastructure related to their project, whether on-site or off-site, will be outlined in the Redeveloper's Agreement with the Borough of River Edge. Off-site responsibility for properties not covered under the redeveloper's agreement will be determined in the same manner as other development projects throughout the Borough of River Edge during the permit and/or site plan review phases.

All infrastructure improvements shall comply with applicable local state and federal codes including the Americans with Disabilities Act. All streetscape improvements shall also comply with applicable standards found in this Plan. All utilities shall be placed underground, unless otherwise authorized by the Borough of River Edge.

8.1.4 Supplementary Submission Requirements

The planned development shall meet the following submission requirements, which shall supplement submission requirements otherwise specified for applications to the Borough of River Edge Planning Board:

- A. A general land use plan at a scale specified by ordinance indicating the tract area and general locations of the land uses to be included in the planned development.
- B. A circulation plan showing the general locations and types of transportation facilities, including facilities for pedestrian access, within the planned development and any proposed improvements to the existing transportation system outside the planned development;
- C. A utility plan indicating the need for and showing the proposed location of sewage and water lines, any drainage facilities necessitated by the physical characteristics of the site, proposed methods for handling solid waste disposal, and a plan for the operation and maintenance of proposed utilities;
- D. A storm water management plan setting forth the proposed method of controlling and managing storm water on the site;
- E. An environmental inventory including a general description of the vegetation, soils, topography, geology, surface hydrology, climate and cultural resources of the site, existing man-made structures or features and the probable impact of the development on the environmental attributes of the site;
- F. A local service plan indicating those public services which the applicant proposes to provide and which may include, but not be limited to, water, sewer, cable and solid waste disposal;

G. A fiscal report describing the anticipated demand on municipal services to be generated by the planned development and any other financial impacts to be faced by the Borough of River Edge as a result of the completion of the planned development;

H. A housing plan outlining the number of housing units, if any, to be provided and the extent to which any housing obligation assigned to the Borough of River Edge will be fulfilled by the development; and

I. A proposed timing schedule in the case of a planned development.

Affordable Housing Component: In no event shall a redevelopment project constructed pursuant to this Plan result in an additional obligation to the Borough of River Edge to provide affordable housing pursuant to policy or regulations of the Council on Affordable Housing unless such additional obligation is met by the developer of such project pursuant to a redeveloper agreement between the developer of such project pursuant to a redeveloper agreement. All costs to the Borough of River Edge, including professional fees associated with assisting the Borough of River Edge in addressing the impact of a redevelopment project under this Plan on the Borough's affordable housing obligation, shall be paid by the developer.

Any deviations from the use requirements or affordable housing provisions of this Redevelopment Plan shall be addressed as an amendment to the Redevelopment Plan rather than via variance relief through the Planning Board of the Borough of River Edge.

8.1.5 Procedure for Amending the Plan

This Plan may be amended from time to time in accordance with the procedures of the LRHL. An appropriate fee, as determined by the Borough, shall be paid by the party requesting such amendment, unless the request is issued from an agency of the Borough. The Borough, at its sole discretion, may require the party requesting the amendments to prepare a study of the impact of such amendments, which study must be prepared by a professional planner licensed in the State of New Jersey.

8.2 PERMITTED USES

The following uses shall be permitted within the Redevelopment Area.

A. Offices: General and professional offices, including, by way of example, but not limitation, offices for doctors', dentists', lawyers' accountants' and architects' offices provided they do not occupy the ground floor. The only exception is if there is a market limitation for retail uses on the provided front street edge. If offices are provided, the front edge (semi-public space) along the street must be landscaped.

B. Retail: Stores, restaurants, and similar facilities including by way of example, but not limitation markets, stores, convenience stores, entertainment establishments, restaurants, cafes, bars, health and fitness clubs, clothing stores, banks and boutique retailers. Outdoor dining is permitted at specified locations and further subject to clear pedestrian passage as specified later in the Design Standards.

C. Parking: Premises built within a structure for uses in the redevelopment area and used exclusively for the parking of automobiles and other vehicles primarily to support those uses. Parking is also permitted, and expected, to be supplied for NJTransit commuters. Parking structures are subject to review for compliance to Design Standards and NJTransit 2005 Guidelines and Standards Manual.

D. Multi-Family Residential: Includes premises available for long-term human habitation by means of ownership or rental, but excluding premises offered for rental for periods of less than a month's duration; excludes all boarding houses and rooming houses.

E. Hotels: Small hotels with a minimum of 75 rooms and with attached structured parking and access to a park, or riverwalk.

F. Live work units: Those units that contain a residential portion of the unit attached or directly accessible to the work portion. There can be a limited number of employees, based on size, outside those that directly inhabit in the residential portion of the unit. The work portion cannot exceed 50% of the total space.

G. Mixed-uses that combine any two or more of the permitted uses

within a single building. The ground floor retail locations are shown in the **Exhibit 8.1**. Along these edges retail and services that have walk-in customers are allowed and encouraged. Offices are allowed on the upper levels with lobby access on the lower levels. If provided at the lower level along the sidewalk edge, a semipublic space of no less than 4 feet shall be required that is extensively landscaped thereby obstructing views from passersby into offices. An appropriate sized access sidewalk shall be provided from the sidewalk to the entrance of these lowered floor offices.

8.3 INTERIM USES

The following shall be permitted interim uses in the Area. The duration during which such uses may remain in place shall be determined by the Redevelopment Entity and shall be reflected within the Redeveloper Agreements as may be executed between the Redevelopment Entity and the designated Redeveloper.

A. Surface parking facilities

B. Any other use approved within the Redeveloper Agreement that will not encumber the property in any way as would hinder the ultimate development of the permanent permitted use

C. Construction Staging Area

8.4 PROHIBITED USES

Any use not expressly permitted as either a permitted use or as an interim use shall be prohibited within the Redevelopment Area. The following list includes examples of uses that are prohibited and is not intended to be exhaustive of all prohibited uses.

A. Drive-through commercial uses where patrons remain in automobiles,

B. Gas and service stations and car wash facilities.

C. Vending machines, except when located within buildings.

D. Commercial kennels and animal husbandry facilities.

E. Warehousing, distribution or storage facilities.

- F. Automotive sales, repair, cleaning or long-term storage or on-grade parking lots.
- G. Junk yards and recycling facilities.
- H. Labor pool buildings, halfway houses, and food pantries.
- I. Industrial and/or manufacturing operations.
- J. Facilities producing adverse impacts on adjacent properties in general, usually as a result of odor, vibration, noise or pollution: excluding NJ Transit rail or bus operations in the redevelopment area.
- K. Structured parking facilities, except those required to meet the permitted parking requirements of permitted uses within the redevelopment area.
- L. Tattoo studios and adult book and novelty shops.
- M. Pool halls and night clubs.
- N. Church, synagogue, house of worship, and similar religious facility.

8.5 LAND USE PLAN

Exhibit 8 illustrates the land use regulation of the Area.

The Land Use Plan within the Redevelopment Area is flexible; however the programmed uses are specific to their respective locations. Within the three buildings, there may be approximately 70,000 square feet of office/flex space, 35,000 square feet of retail/commercial space, approximately 130 housing units based on an average unit size of 1,250 square feet (including circulation and mechanical elements) and approximately 1050 parking spaces. The program is recommended to optimize the amount of activity necessary to activate the plaza, pedestrian realm and adjacent areas. The program is approximate and will be market driven. For the plan to be successful and implementable it is important to have a diverse range of usages.

| Use | Amount |
|-------------------------|----------------------|
| Retail | approx. 35,000 sq ft |
| Office and/or Live-Work | approx. 70,000 sq ft |
| Residential | approx. 130 units |
| Parking | approx. 1050 spaces |

The following chart denotes the minimum size of residential units by unit type. Furthermore, the practice of residents' buying multiple adjacent units for the purpose of combining them as one larger unit is encouraged.

| Unit Type | Minimum Size (per unit) |
|-----------|-------------------------|
| Studio | 500 sq ft |
| 1 Bedroom | 750 sq ft |
| 2 Bedroom | 900 sq ft |

Live-Work units shall be created as the market demands. When built, these units must first occupy the the lower floors of Building #2, as noted in **Exhibit 8.3**. However, if there is a desire for additional Live-Work units, they may be located in any of the buildings, given that these units do not share circulatory elements (hallways, elevators, stairs, entrances, etc.) with purely residential units.

To maintain the desired character of the Area, retail must be located at the ground floor at the locations specified in **Exhibit 8.1**.

Because each building will contain several uses, it is important to recognize the delineation of entries, for the residential, retail and office uses. Entries shall be separate from one another, and where necessary, security front desks be implemented. Prominent (primary) entries to the residential units shall feature a marquee extending out onto the sidewalk. See Exhibit 17 to see how the building edge interacts with the pedestrian realm and street.

None of the newly constructed dwelling units in the redevelopment project shall consist of dwelling units containing three (3) or more bedrooms.

Exhibit 8.1 Land Use Plan – Retail
(ground floor)

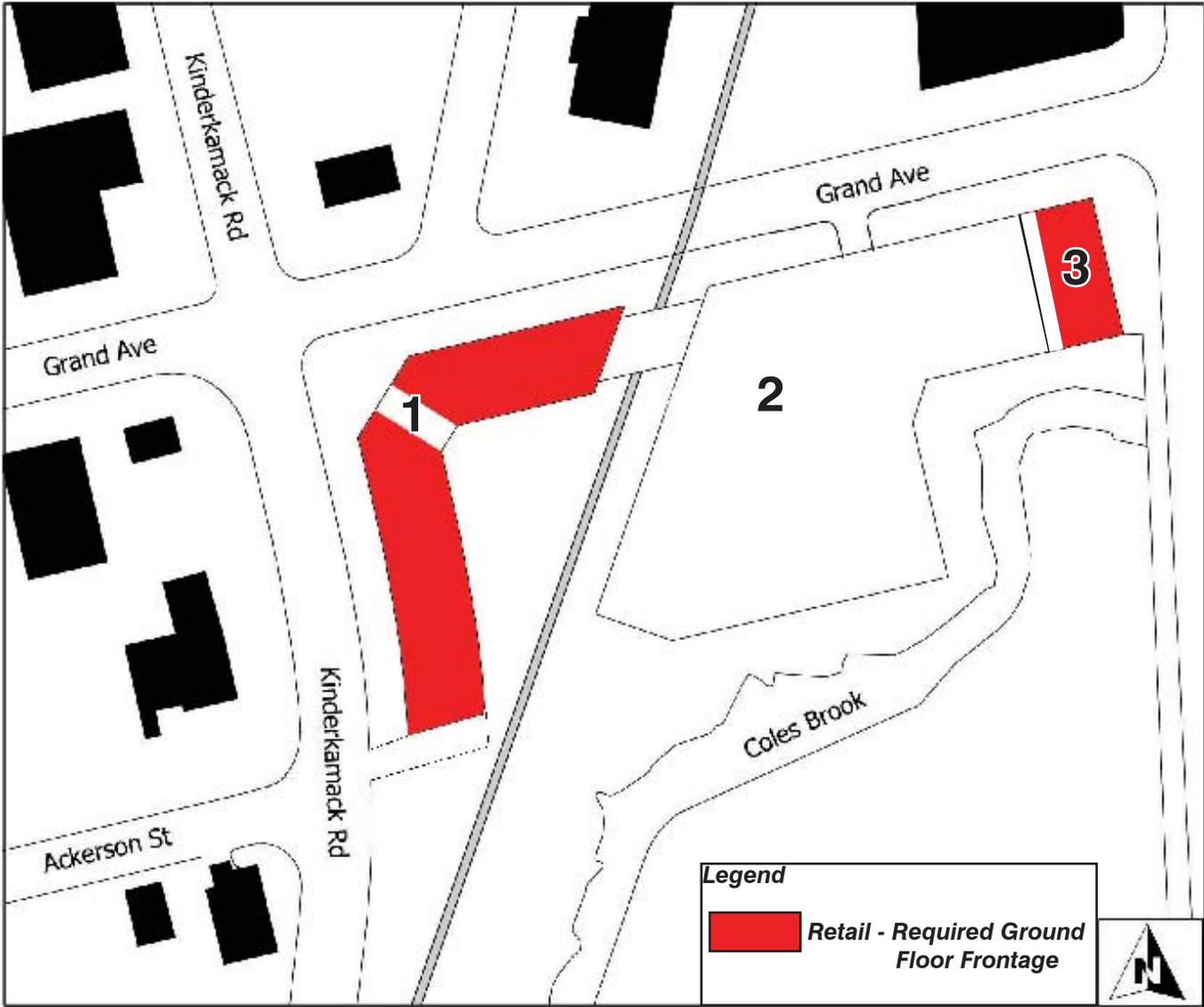
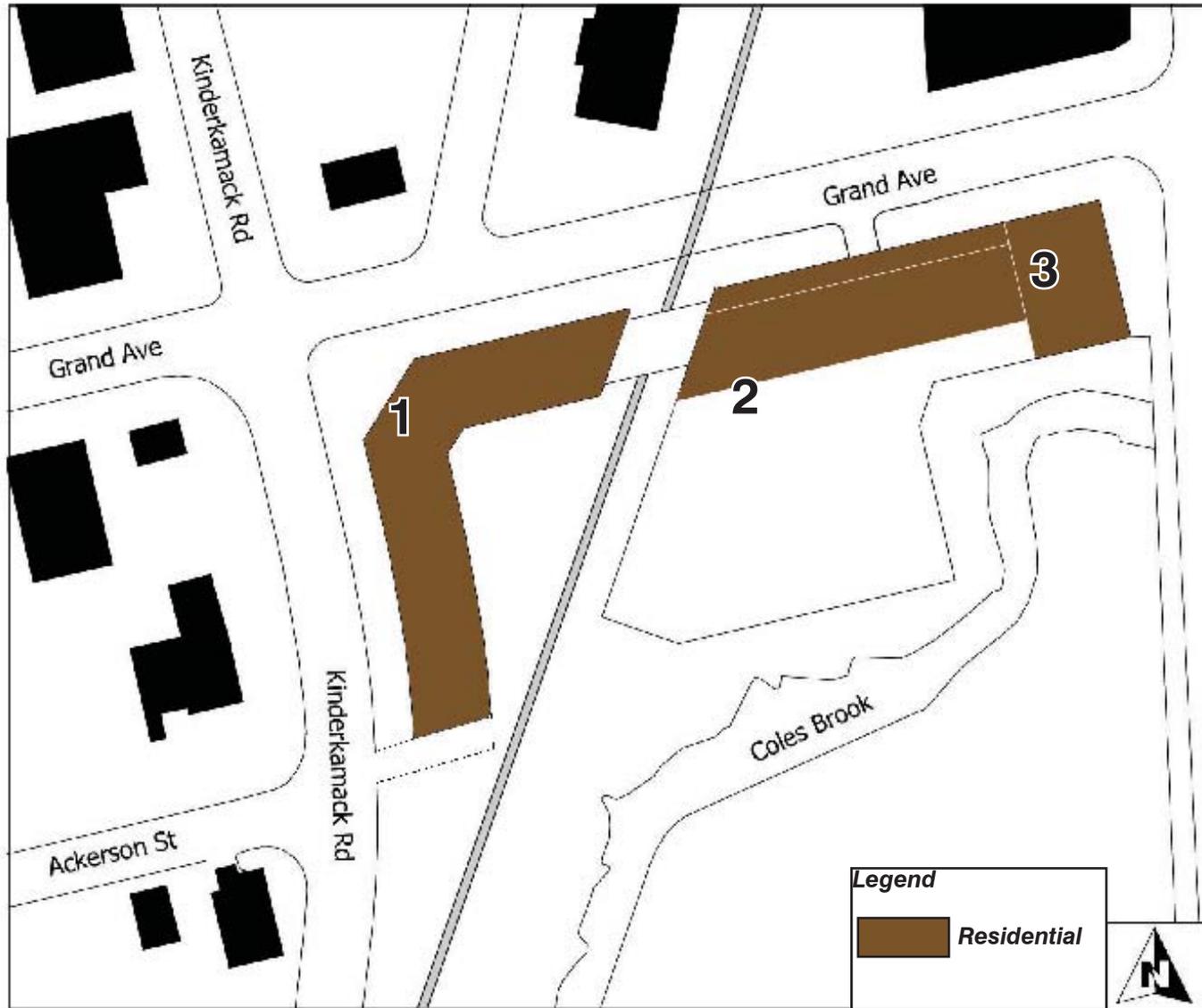
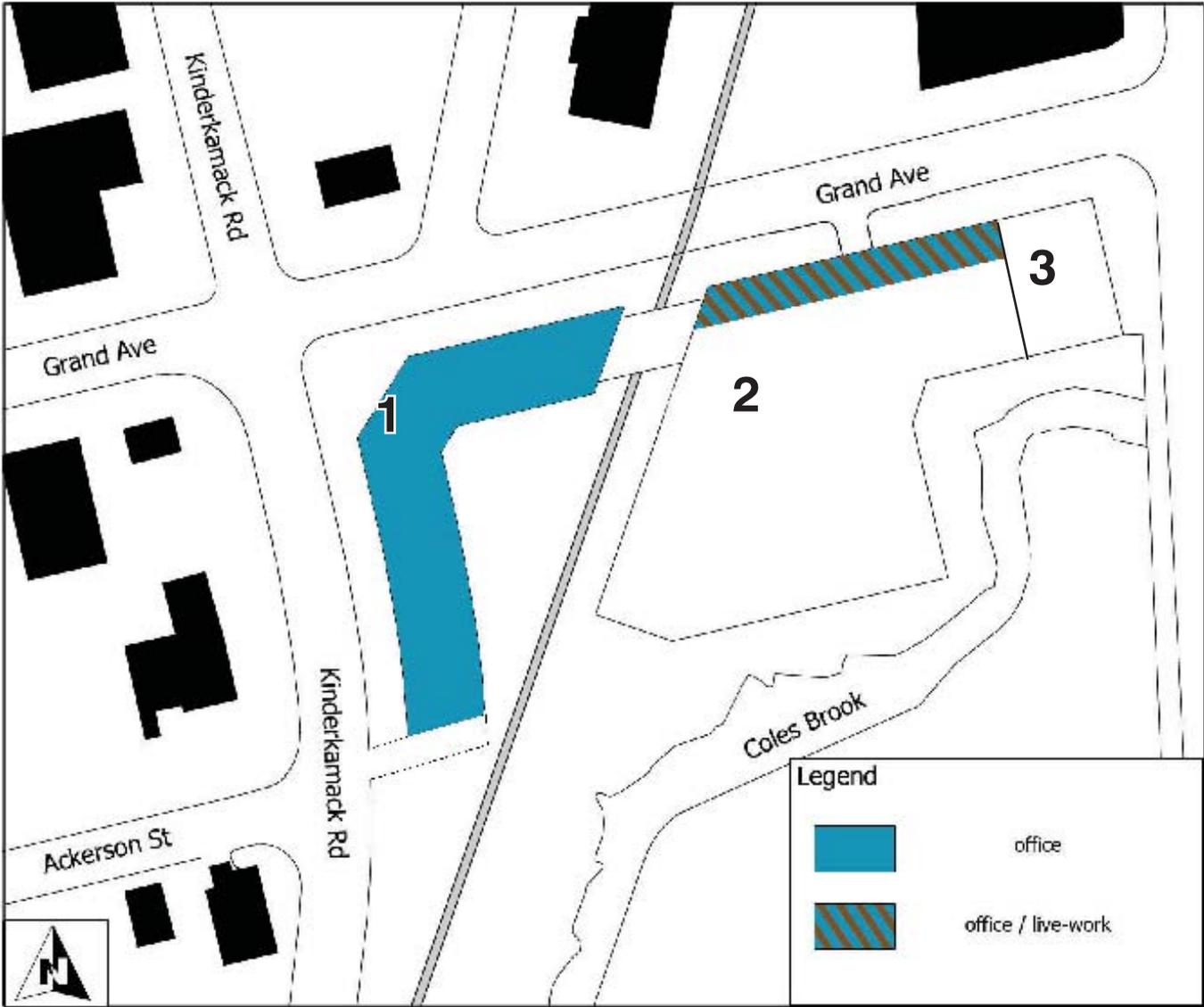
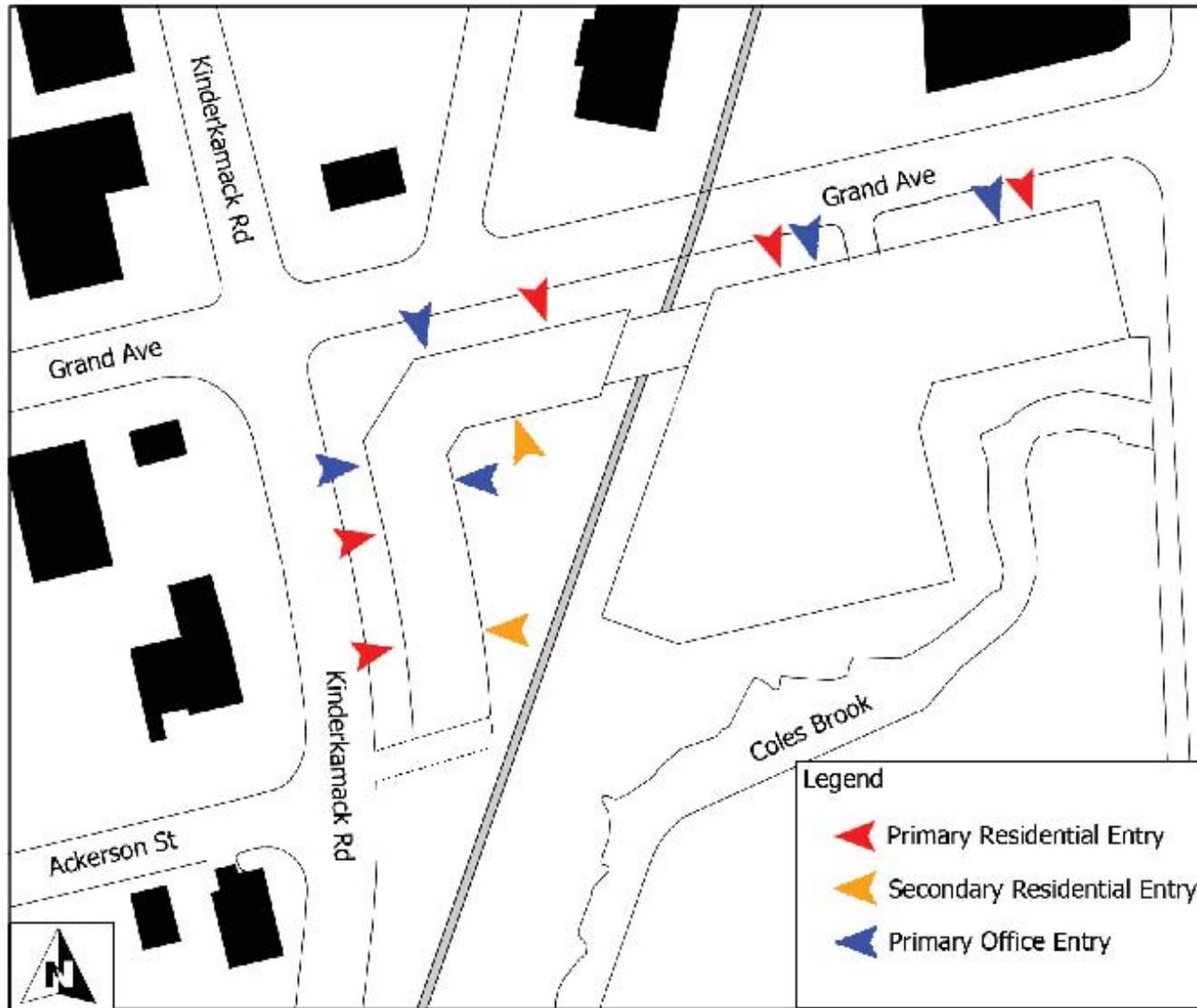


Exhibit 8.2 Land Use Plan – Residential
(above ground floor)







BUILDING REGULATIONS

9.0 BUILDING REQUIREMENTS

9.1 DEVELOPABLE PARCELS

The Area has been divided into two (2) development parcels. Up to three (3) separate buildings are allowed on these parcels. The location of each parcel is illustrated in Exhibit 3. Each parcel has been color coded. The three buildings have been identified in the Figure Ground Plan, **Exhibit 12.0**.

The development regulations for each parcel are based on a form-based regulation that determine the character of the spaces, defined by the streets, pedestrian realm along with the mass, form, detailing and materiality of the buildings.

There are two sets of fundamental regulations within this document that control the visual and spatial character: the Building Regulating Plan and the Architectural and Landscape Standards. These regulations have sufficient flexibility for the architects to design great buildings and streetscapes while insuring the minimum level of appropriate urban design standards. Included in the basic standards for a form base code are the buildable footprint, build-to-lines, encroachments, setbacks and step backs, identifiable building widths, bay spacing, bulk and height reflected through building sections and applied to the recommended street network and minimum requirements of the Transportation and Pedestrian Circulation Plans.

9.2 ENVIRONMENTAL CONDITIONS

9.2.1 Building in the 100 year floodplain

A design challenge presented by this site is its relation to the 100 year floodplain. **Exhibit 10.0** illustrates the topography and its relationship to the floodplain. The original map was generated by Christopher P. Statile, P.E. Professional Engineers & Planners, in association with Mega Engineering, Inc. and Amy S. Greene Environmental Consultants for NJTransit in January 2006.

On this map, the 100 year floodplain is at the 9 foot elevation. Topographic lines in red indicate areas of the site in the floodplain while green lines indicate areas out of the floodplain. In the 100 year floodplain, no more than 20% of the site may be filled, and use restrictions are complicated. As a design solution the bottom of the first structural floor is at the 10 foot elevation, consequently leaving the first finished floor at the 11 foot elevation. Additionally, a dual-level sidewalk is recommended as a means to carry the retail-pedestrian experience above the floodplain elevation.

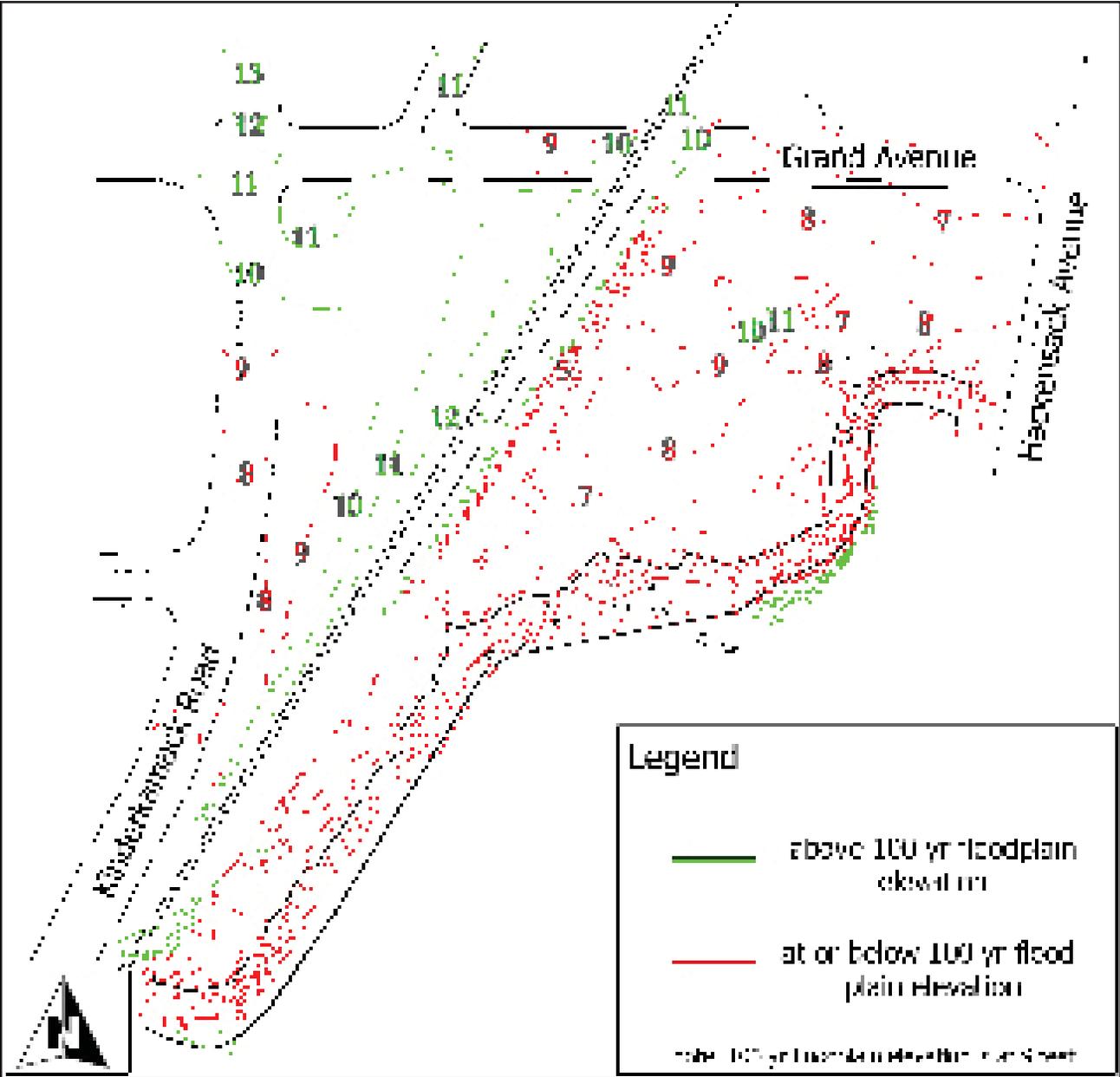
By building this way, the only fill in the floodplain would consist of the footings, beams and piles. Additional fill would result from the steps of the dual-level sidewalk. This technique minimizes disturbance of the floodplain, allowing flooding waters to flow underneath the building as needed. See **Exhibits 14.1-14.4** for cross-sectional diagrams illustrating this strategy.

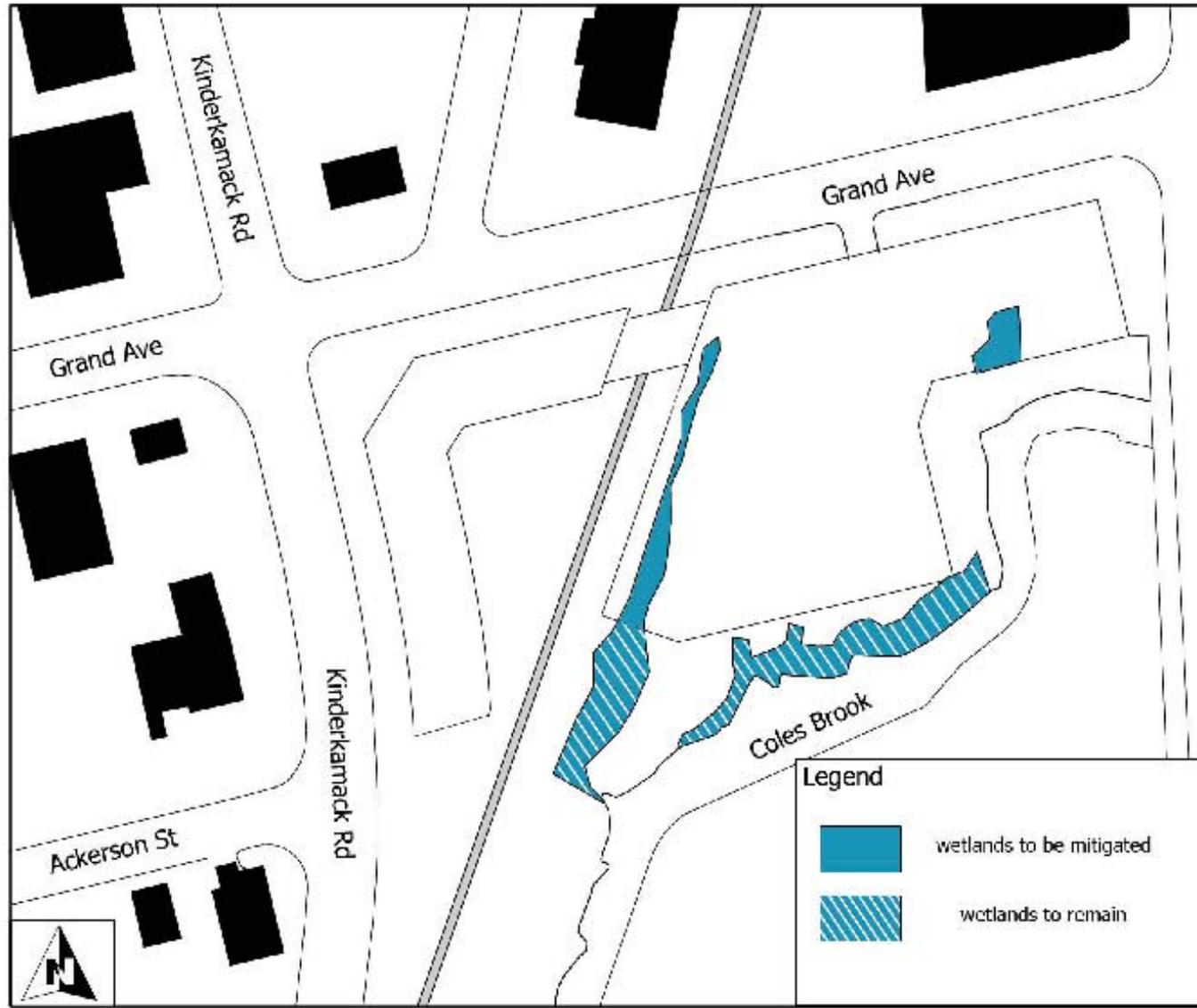
9.2.2 Wetlands Mitigation

In addition to floodplain issues, the Area also holds three wetlands areas, illustrated in **Exhibit 11.0**. In total, there is 14,560 square feet of wetlands. Due to the proposed redevelopment, 3,736 square feet of wetlands must be mitigated. Considering the amount of open space left available on the site, these 3,736 square feet may likely be mitigated within the Area.

9.2.3 Stormwater Management

Stormwater run-off will be captured, as much as possible, by the green-roofed parking structure and rainwater-collecting cisterns on other roofs. The rainwater should be reused within the Area, to irrigate the plaza and other site landscaping. If the green roof technique is not used, other means of stormwater management must be developed and approved by the Planning Board.





9.3 BUILDING REGULATIONS

The following summarizes the regulations for the buildings. Each development parcel has a maximum developable area or building footprint, a build-to-line, setbacks and stepbacks, and height and bulk standards specified.

9.3.1 Figure Ground Plan / Developable Area

The figure ground plan / developable area regulation is **Exhibit 12.0**. This illustrates the allowable building footprint of the ground floor of the building. It is within this designated area, on each parcel, that the building can be located.

9.3.2 Build-to-line

The build-to-line is the line (typically parallel to the property line) along which the primary façade of the building must be located. Build-to-lines are determined to ensure that building placement clearly defines street edges and corners. The build-to-line allows flexibility to the articulation of the facade allowing portions of the facade to deviate in limited increments from this line unless specified otherwise. Deviations using recessions along the facades are recommended to create identifiable building widths. The Build-to-lines are illustrated in **Exhibit 13.0**, which locates the build-to-lines for all parcels in the Redevelopment Area.

Along Kinderkamack Road, and Grand Street, to promote the identifiable building widths and to express the facade as a series of connected smaller buildings there can be deviations from the build-to-line of up to six feet back of the build-to line and two feet in front of the build-to-line at the locations of the various identifiable building widths. On the other facades, deviations are limited to four feet either in front or in back of the build-to-line. The build-to-line regulations assure that an appropriate street-wall and building articulation is formed. In this plan, the build-to-line is illustrated for each development parcel.

The primary lower facade before the stepbacks must respect the build-to- line. At street corners, building facades must occupy 100% of the build-to-line within 50 feet of the property corner.

9.3.3 Stepbacks and Setbacks

Stepbacks are a required design feature of all facades. See **Exhibits 14.1-14.4**. A stepback is where the building must be stepped back from a lower facade of a building along the block at a prescribed range of heights. This is required at the fourth or fifth story to insure that the scale of the buildings and the proper proportions

of the street space are respected. The three to four story stepback insures that the “small town” pedestrian scale is respected. The stepback, along with the identifiable building width, is required to create the more intimate sense of scale and a small town feeling at the lower levels, allowing a building to increase in height in back of this line, minimizing the visual impact while assuring the development’s financial feasibility. Step backs must occur at the recommended range of heights, along with the identifiable building widths and bay spacing. Stepbacks must meet the minimum stepback requirements and must be terminated by a predominant cornice.

Buildings within the Redevelopment Area must employ the stepback thereby reflecting, particularly from the ground view, the continuity of the traditional pedestrian scaled lower building facades. With the stepback, and using the identifiable building widths and the traditional bay spacing, the scale and rhythm of traditional buildings are respected which are comfortable and considered positive by pedestrians. Above the stepback, upper floors are less visible and apparent when the step back is applied.

Stepbacks also reduce the mass of the new buildings and open more of the street to sunlight. Additionally, stepbacks provide flat areas that can be used as terraces creating another amenity for the residents while generating a building that is more ecologically responsive.

The stepback is required in specified locations and is illustrated on the building sections. The stepbacks for each of the buildings are illustrated for each parcel. The stepback regulations are drawn to the maximum number of stories of the building. If a building has a lower number of stories, the lower portion of the section apply. On landmark corners the stepback is optional.

A setback is the minimum required number of feet that a building must set back from a specified line, be that a property line, right-of-way line, curb line, build-to-line or a step back line. Setbacks are specified in **Exhibits 14.1-14.4**.

The only exception to the stepback requirement is Building 2. Due to the large amount of parking required within the structure and the desire to have the parking garage hidden from street view, Building 2 may have its first stepback at the seventh floor.

SECTION 9.0 BUILDING REGULATIONS

9.3.4 Height and Mass

Height is expressed in the number of stories independent of the range of floor to floor dimensions. The tallest building in the plan, Building 3, has a maximum number of floors is twelve (12). All other building or portions of buildings heights are illustrated in **Exhibit 14**.

Floor height can and should vary. This is also illustrated in **Exhibits 14.1-14.4**. Typically the ground floors are higher (taller), the middle floors are lower and the top floors are again higher. The building sections indicate those floors that can have additional height. This is encouraged to add interest and greater marketability to the building. The height is limited to a number of stories above the adjacent ground/sidewalk grade. The following chart regulates the floor heights by regulating the floor-to-ceiling height. The floor-to-ceiling height is the vertical distance from the finished floor to the ceiling. In instances of architectural expression at the roof line, such as a curved roof (shown in **Exhibits 14.1-14.4**), or in instances where residents buy vertically adjacent multiple units for the purpose of creating one larger unit, the regulations given in this chart may be amended pursuant to Planning Board approval.

Regulations for Floor-to-Ceiling heights per use and building:

| Floor Type | Building #1 | Building #2 | Building #3 |
|--|-------------------|-------------------|-------------------|
| Ground Floor Retail | 9 feet to 15 feet | n/a | 9 feet to 15 feet |
| Ground Floor Office | n/a | 9 feet to 12 feet | n/a |
| Non-Ground Floor Office | 9 feet to 12 feet | 9 feet to 12 feet | n/a |
| Upper Residential Floors (top 3 floors)* | 9 feet to 15 feet | 9 feet to 15 feet | 9 feet to 15 feet |
| All other Residential Floors** | 9 feet to 12 feet | 9 feet to 12 feet | 9 feet to 12 feet |

*includes residential floor in Building #2 that fronts onto the green roof of the parking deck. If the green roof is not built, this floor shall be treated as all other residential floors.

**includes Non-Ground floor Live-Work units

The mass of the buildings are determined by a combination of height, building

length, stepbacks and bay spacing and upper cornice relation to the sky. For the purposes of establishing height and mass, the proposed buildings shall exhibit characteristics of height, step back, and mass within the expressed building footprint that are consistent with the parameters set forth in this section. The mass of the building(s) can be modulated by the use of roof forms and parapet treatments, materials and colors. It is highly recommended that the lower floors of the buildings (below the first stepback) be masonry, while the upper floors (above first stepback) be expressed in lighter, more transparent materials, thereby reducing the apparent mass of the buildings.

9.3.5 NJ Transit Construction Standards

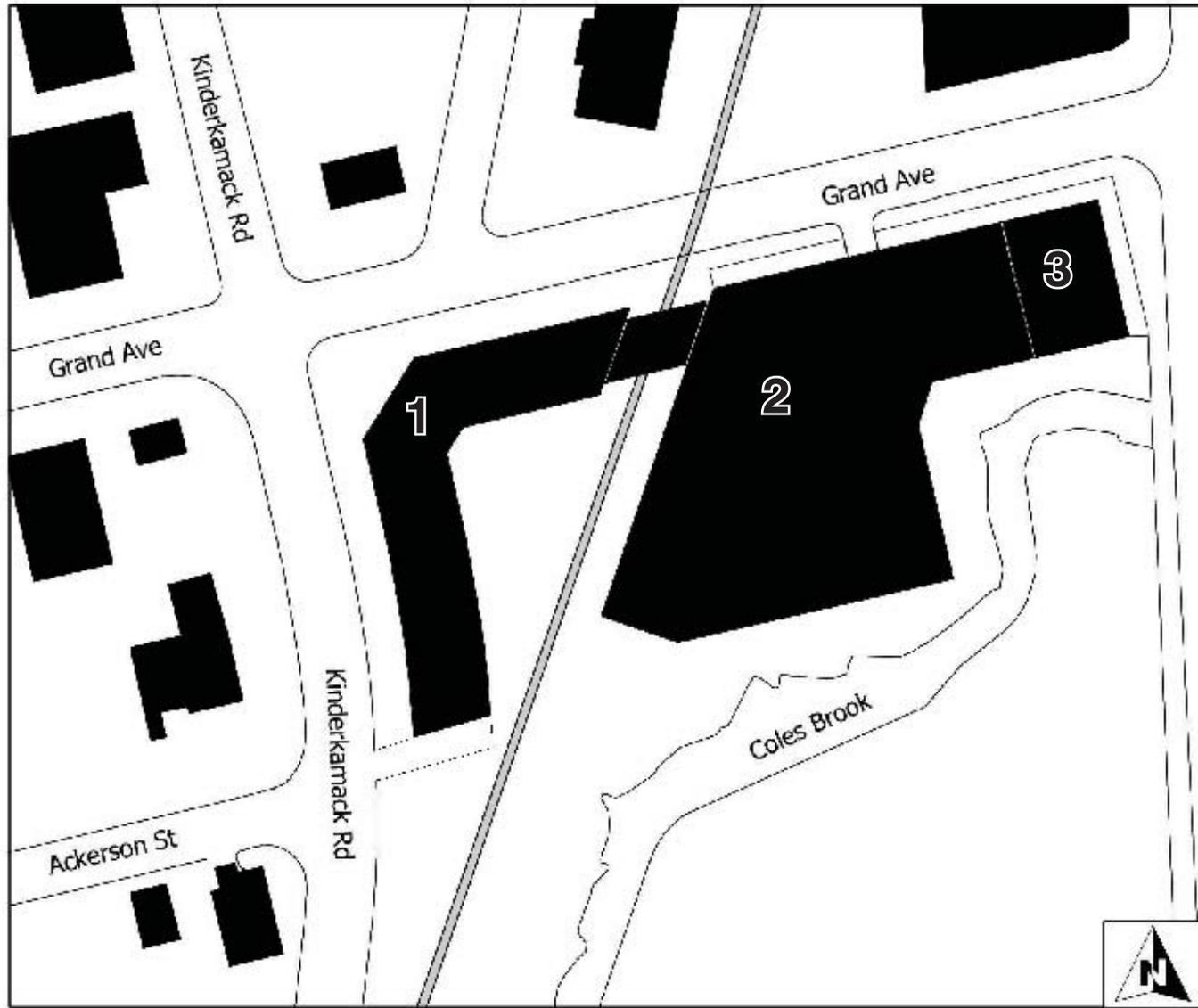
Along the Pascack Valley Rail Line, a minimum sixty (60) foot wide ROW must be maintained. In addition, all in-field site activities impacting NJ Transit operations must be coordinated with and approved by NJ Transit's Real Estate and Economic Development and Rail Operations units.

9.3.6 Residential Density

Each redevelopment project may be conditioned upon a negotiated maximum residential density, to be negotiated between the redevelopment entity and the designated redeveloper. The maximum permitted residential density shall be specifically set forth in an executed redeveloper's agreement prior to a development application being deemed complete for review before the Planning Board. The maximum permitted residential density shall include all housing units affordable to low- and moderate income household that are provided in accordance with this plan's provisions relating to affordable housing.

9.4 EXHIBITS OF BUILDING REGULATIONS

The following pages set forth the development regulations for each building.



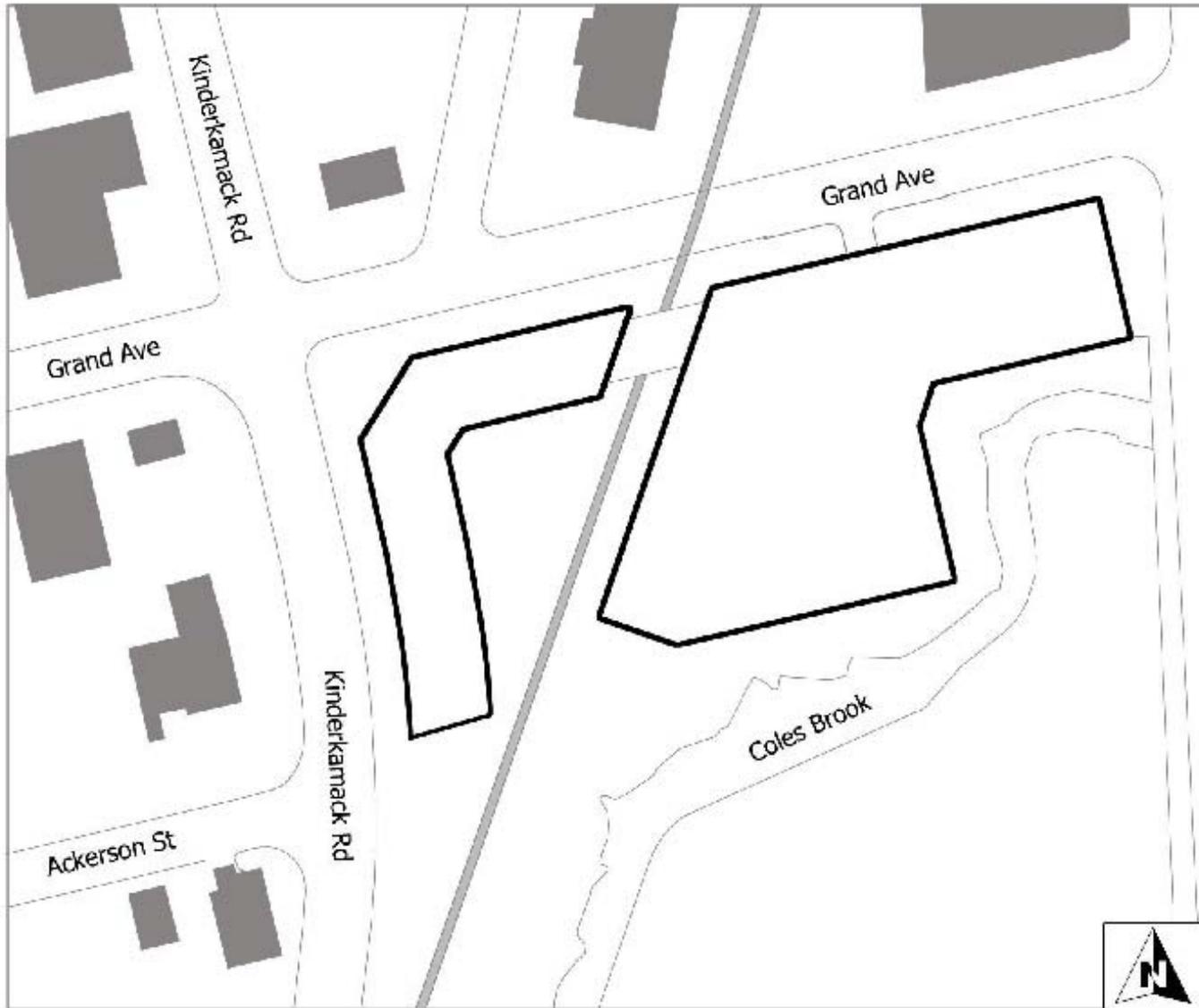


Exhibit 14.1

Building Regulations – Section AA (Building 1)

| DETAILS | ID | INFORMATION |
|---------------------------|-----|---|
| SITE | | |
| Section Number | x | AA |
| Grade Change | GC | 1.05% (Approximate) |
| PUBLIC | | |
| Parking | P | 8 feet wide |
| Sidewalk | SW | 10 to 14 feet wide |
| Planter Wall | PW | 6 feet wide |
| Elevated Sidewalk | ESW | 10 to 14 feet wide (elevated 2 feet) |
| Public Plaza | PP | 100 feet (variable) |
| BUILDING | | |
| Building Type | n/a | Mixed-Use with Retail on Ground Floor |
| Building (Depth) | B | 60 feet |
| Circulation 1 | C1 | 10 feet |
| Circulation 2 | C2 | n/a |
| Building Height | BH | 6 Stories, up to 74 feet (14 foot ground floor) |
| Optional Building Height | OBH | 6 Stories, up to 86 feet (14 foot ground floor) |
| Stepback | ST | 15 feet |
| Rear Stepback | RST | 10 feet |
| Lower Encroachment | LE | Awning 8 feet, Balcony/Bay Window up to 3 feet |
| Upper Encroachment | UE | Balcony up to 3 feet |
| Rear Lower Encroachment | RLE | n/a |
| Rear Upper Encroachment | RUE | n/a |
| Special Consideration | SC | n/a |
| Embedded Parking | EPA | n/a |
| Rear Parking Encroachment | RPE | n/a |
| Green Roof | GR | Highly Recommended (low level plantings) |

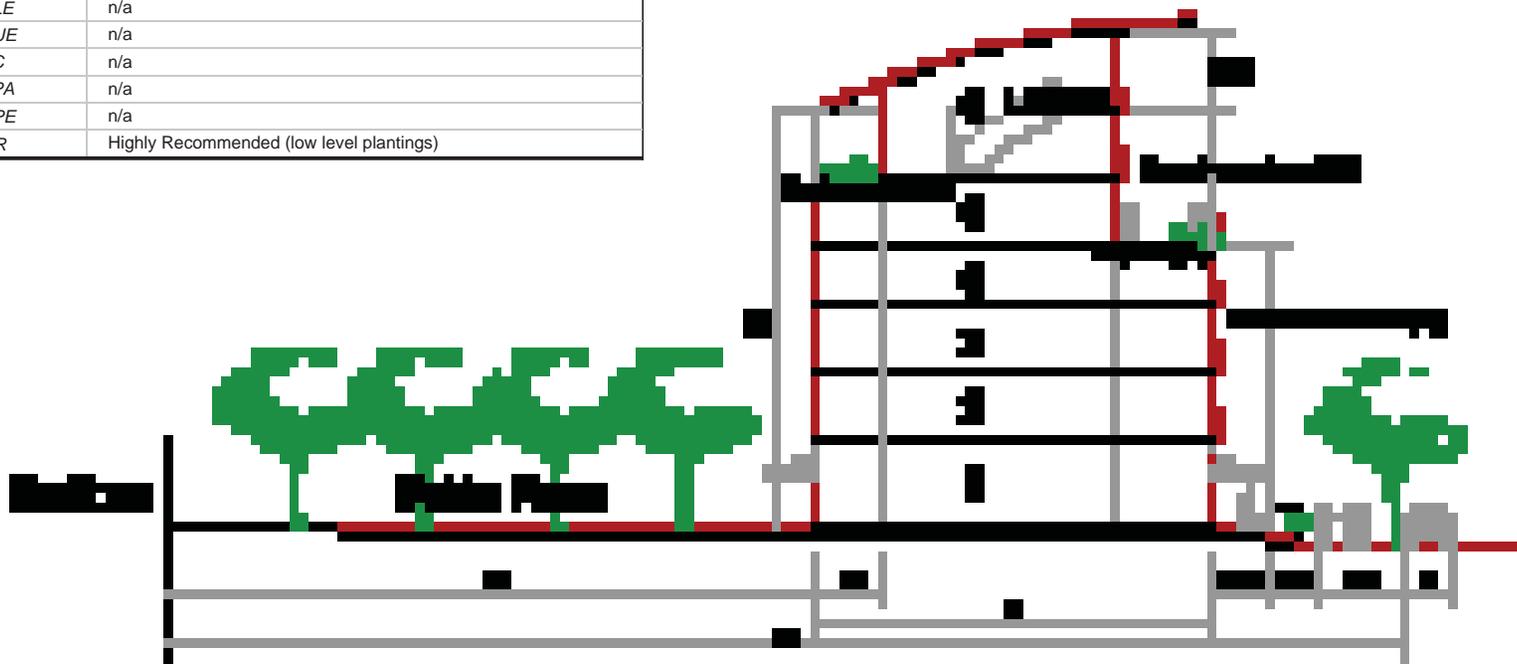
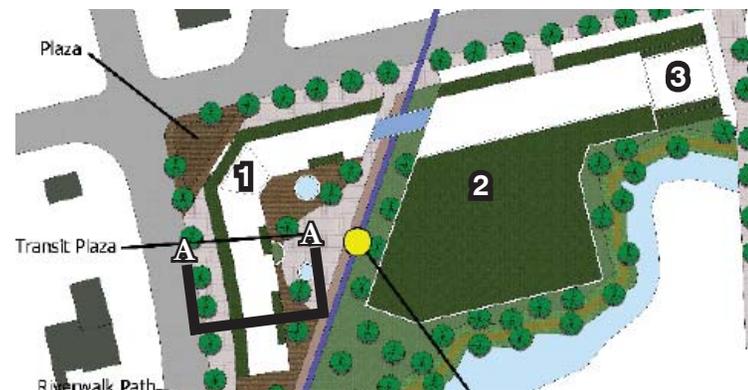


Exhibit 14.2 Building Regulations – Section BB Special Consideration (Building 2)

| DETAILS | ID | INFORMATION |
|---------------------------|-----|---|
| SITE | | |
| Section Number | x | BB (Special Consideration) |
| Grade Change | GC | 1.18% (Approximate) |
| PUBLIC | | |
| Parking | P | 8 feet |
| Sidewalk | SW | 10 to 14 feet |
| Planter Wall | PW | 6 feet |
| Elevated Sidewalk | ESW | 10 to 14 feet (elevated 2 feet, as much as 4 feet) |
| Public Plaza | PP | 195 feet (on top level of parking structure) |
| BUILDING | | |
| Building Type | n/a | Mixed-Use with Office on Ground Floor |
| Building (Depth) | B | 75 feet with 30 foot liner building adjacent |
| Circulation 1 | C1 | 10 feet |
| Circulation 2 | C2 | 5 feet |
| Building Height | BH | 9 Stories, up to 110 feet with penthouse (10 foot ground floor) |
| Optional Building Height | OBH | n/a |
| Stepback | ST | 15 feet (with plantings) |
| Rear Stepback | RST | 10 feet (with plantings) |
| Lower Encroachment | LE | Balcony/Bay Window up to 3 feet |
| Upper Encroachment | UE | Balcony up to 3 feet |
| Rear Lower Encroachment | RLE | n/a |
| Rear Upper Encroachment | RUE | Balcony up to 3 feet |
| Special Consideration | SC | See Diagram |
| Embedded Parking | EPA | 6 Levels @ 240 feet (variable, see plan view) |
| Rear Parking Encroachment | RPE | Optional balcony up to 3 feet |
| Green Roof | GR | Highly Recommended (low level plantings) |

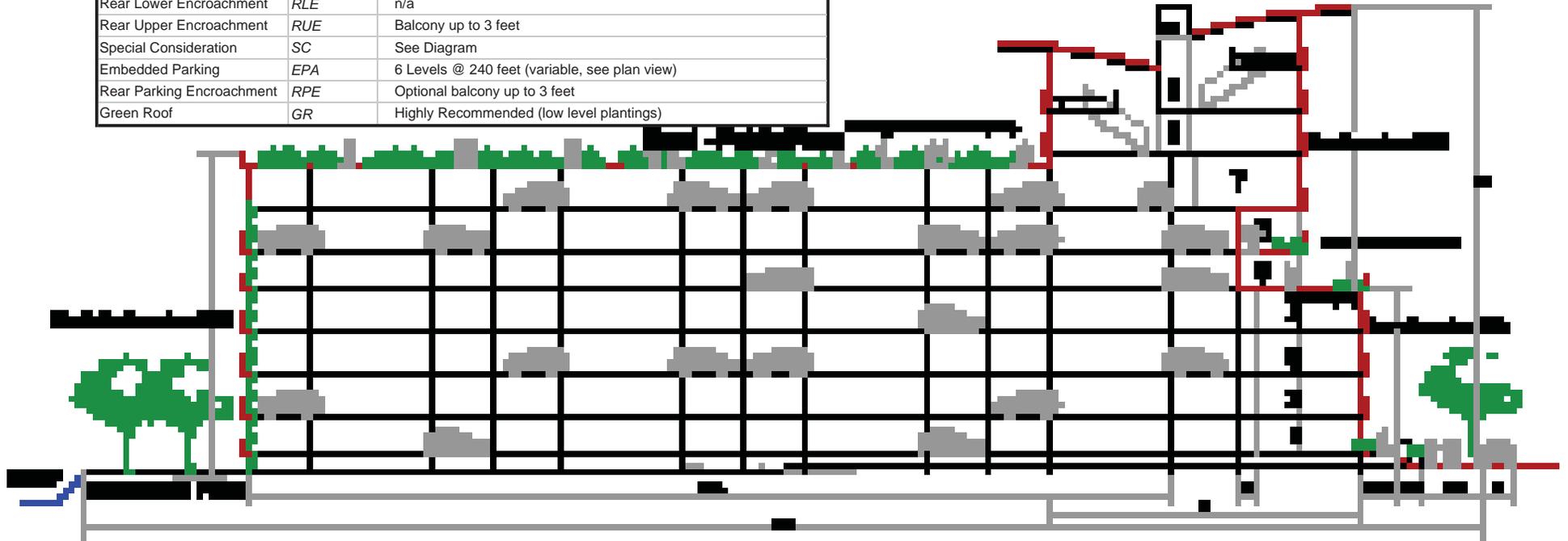
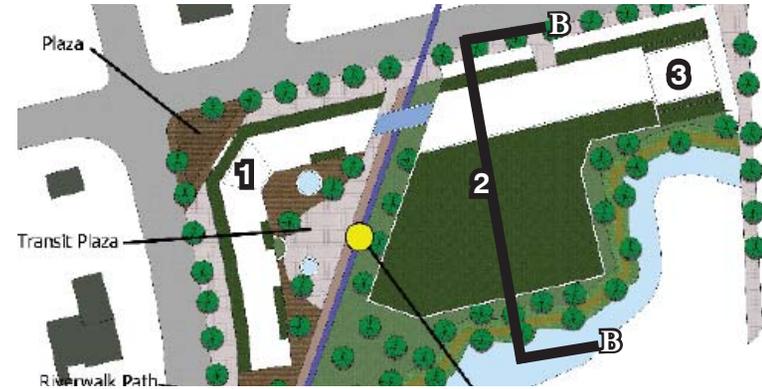


Exhibit 14.3 Building Regulations – Section BB (Building 2)

| DETAILS | ID | INFORMATION |
|-------------------|-----|---|
| SITE | | |
| Section Number | x | BB |
| Grade Change | GC | 1.18% (Approximate) |
| PUBLIC | | |
| Parking | P | 8 feet wide |
| Sidewalk | SW | 10 to 14 feet wide |
| Planter Wall | PW | 6 feet wide |
| Elevated Sidewalk | ESW | 10 to 14 feet wide (elevated 2 feet, as much as 4 feet) |
| Public Plaza | PP | 195 feet (on top level of parking structure) |
| BUILDING | | |
| Building Type | n/a | Mixed-Use with Office on Ground Floor |
| Building (Depth) | B | 75 feet with 30 foot liner building adjacent |
| Circulation 1 | C1 | 8 feet |
| Circulation 2 | C2 | 5 feet |

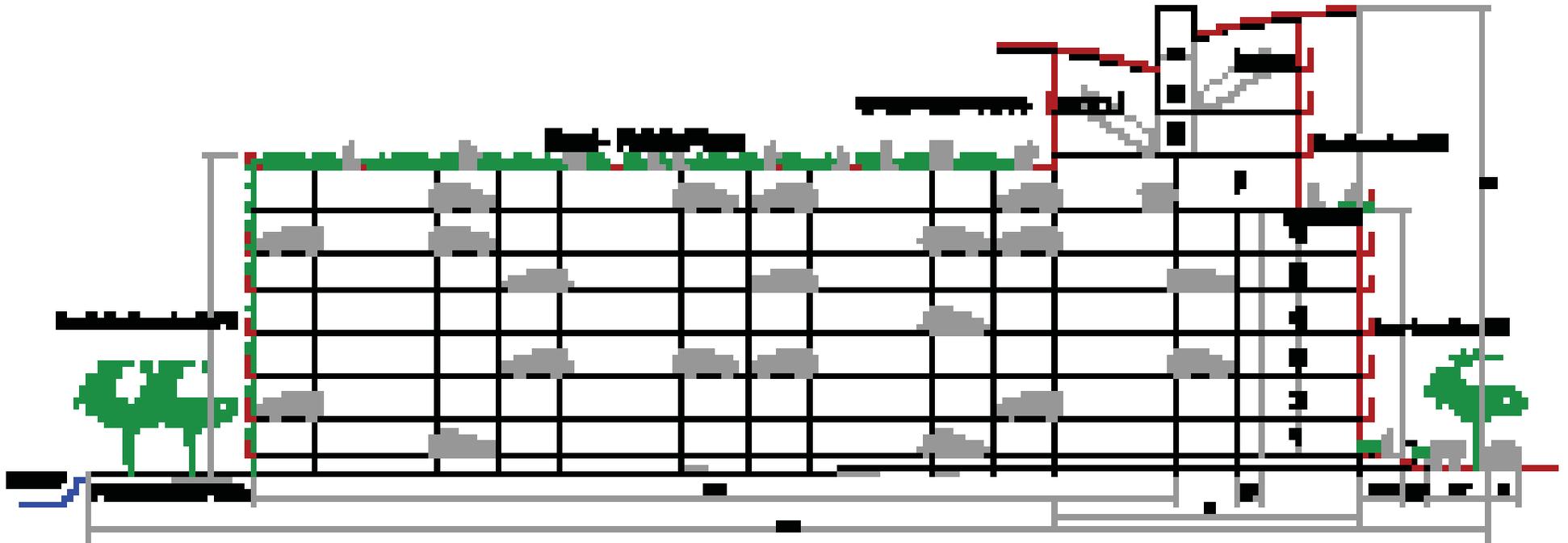
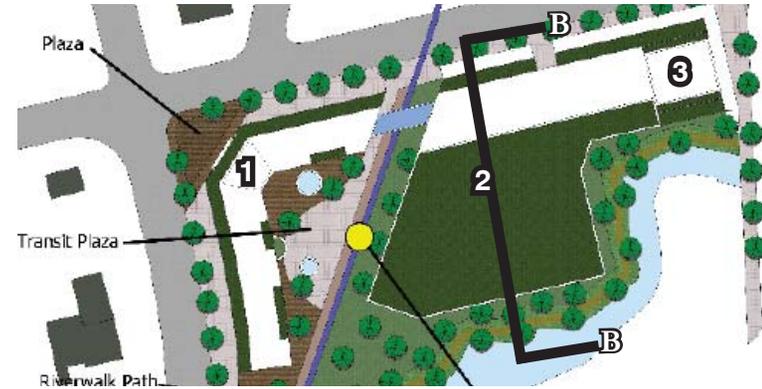


Exhibit 14.4 Building Regulations – Section CC (Building 3)

| DETAILS | ID | INFORMATION |
|---------------------------|-----|--|
| SITE | | |
| Section Number | x | CC |
| Grade Change | GC | 1.21% (Approximate) |
| PUBLIC | | |
| Parking | P | 8 feet wide |
| Sidewalk | SW | 10 to 14 feet wide |
| Planter Wall | PW | 6 feet wide |
| Elevated Sidewalk | ESW | 10 to 14 feet wide (elevated 3 feet, as much as 4 feet) |
| Public Plaza | PP | n/a |
| BUILDING | | |
| Building Type | n/a | Mixed-Use with Retail on Ground Floor |
| Building (Depth) | B | 106 feet |
| Circulation 1 | C1 | 8 feet |
| Circulation 2 | C2 | n/a |
| Building Height | BH | 12 Stories, up to 143 feet with penthouse (14 foot ground floor) |
| Optional Building Height | OBH | n/a |
| Stepback | ST | 15 feet |
| Rear Stepback | RST | 10 feet |
| Lower Encroachment | LE | Balcony/Bay Window up to 3 feet |
| Upper Encroachment | UE | Balcony up to 3 feet |
| Rear Lower Encroachment | RLE | Balcony up to 3 feet |
| Rear Upper Encroachment | RUE | Balcony up to 3 feet |
| Special Consideration | SC | n/a |
| Embedded Parking | EPA | n/a |
| Rear Parking Encroachment | RPE | n/a |
| Green Roof | GR | Highly Recommended (low level plantings) |

