

PART 2 - IF THIS PAGE IS PRINTED

DISCARD IT

# DESIGN STANDARDS

## 10.1 MANDATORY ARCHITECTURAL STANDARDS

The following standards shall apply to all buildings to be constructed in the Area. To the extent that the development of any parcel is to be undertaken in phases, the mandatory or permitted standards for each building applies to each phase and must conform to the design standards provided in this Redevelopment Plan.

### 10.1.1 Architectural Review by Borough

The Buildings' proposed design and associated street improvements are subject to review by the Borough to insure the appropriate historical perspective, as envisioned by the New Bridge Landing State Park, is represented and accommodated to the greatest extent feasible.

### 10.1.2 Identifiable Building Width (Bay Spacing)

Building facades longer than 30 feet must be divided into identifiable building widths (IBW) with the building widths further divided into bays. An identifiable building width applies to long facades that must be designed to look like a series of individual buildings next to each other. The bay is a subdivision of the distance between structural elements of an IBW reflected on the facade. See **Exhibit 16.0**. Traditional town center type buildings are narrow, ranging from 15 to 50 feet. Generally, the entire facade of one of these narrow buildings is one identifiable building width, with that facade subdivided into bays. Typically, on historic buildings, the facade is a unique and complete composition by itself with a base, middle or top. The emphasis is on verticality. This is required on all new facades visible from a street or public space.

The facade of the parking structure adjacent to the Coles Brook must have an attached screening/trellis with plant species that will mask the facade in green.

The street-wall visual character is most positive when separate buildings are immediately adjacent to each other, with a similar build-to-line and unique architectural character. The entire composition of the street-wall based on these design features and the lower floor stepbacks will create a very human scale that is visually interesting. The buildings in this plan must reflect this character. In classic downtown areas, the IBW spacing ranges from 15 to approximately 50 feet wide. These bays historically were individual buildings with separate structural side walls. Each of these buildings had a coherent facade treatment from the ground to the upper cornice or parapet, expressing its' individuality through some combination

of the size and rhythm of the windows (vertical), variation in the height, color and pattern of the brick work, and the treatment of the cornice and the roof. When placed next to each other and sharing a build-to-line they create a harmonious whole, while still reflecting a level of individuality. This is a fundamental urban design characteristic which must be respected in the Area. When mixed-use residential buildings deviate significantly from the basic IBW module and a more consistent facade treatment is used, the building takes on an institutional look that lacks human scale and character for a town center. A modern building can emulate these standards as still look and function as a modern building.

Modern buildings are typically larger in scale and mass than historic buildings. Therefore to create a compatibility with the IBW rhythm of the traditional streetscape, the expression of the IBW must be apparent in the architecture of the proposed buildings. Emphasis must be on verticality not on horizontal features.

Each IBW with its combination of bays shall be expressed in the color of the materials and textures that create a vertical harmony. No long horizontal string courses are allowed.

**Building #3 is not subject to this regulation (10.1.2)**

### 10.1.3 Landmarks

Taller buildings and architectural features on corners of important buildings or at major locations of visual termination, act as landmarks providing legibility and interest to a street, plaza or walking experience. The corner treatment of any building in this Area must meet specified standards. Based on their location, they are illustrated as major and minor architectural landmarks as illustrated on **Exhibit 15.0**. They must have additional height or embellishments and must be contained within the specified areas illustrated. Multifaceted roof shapes for the corners designated as landmarks are required. Regulations providing a building envelope for landmark features are provided in **Exhibit 16.0**.

**Building #3 is not subject to this regulation (10.1.3)**

### 10.1.4 Encroachments

An encroachment is an area beyond the build-to-line into which certain building elements can protrude. The encroachment is expressed in feet by type allowed. Additionally, the story at which encroachments are allowed varies by the nature of the building edge. There are multiple building edges that are specified for each building face. See **Exhibits 14.1-14.4**.

## 10.1.5 Cornices

Cornices are required to emphasize the identifiable building widths or a combination of bays. At each stepback a decorative cornice is required. The decorative cornice will be combined with a parapet at each stepback or terrace level. The parapet height from the inside from the floor to the top of the cornice shall be a minimum of three (3) feet. Each cornice shall be distinctive along the edge of the stepbacks as well as the upper cornice at the skyline to reflect the identifiable building spacing from the ground level to the skyline. Long building walls with flat roofs must appear as a series of individual side by side buildings, each with distinct cornices.

## 10.1.6 Roof Lines

The tops/roofs of buildings must have a range of expressions. The tops of buildings are important design elements for the character of a building(s). To enhance the visual and spatial qualities of the buildings, variation of roof lines is required. A combination of flat roof with decorative cornice/parapet with difference step back lines and occasionally a pitched roof will modify the mass and scale of buildings, creating variety and visual delight.

## 10.1.7 Curved Roofs

Curved roofs are allowed. Standards for curved roofs are illustrated in **Exhibits 14.1-14.4**. If used these roofs must be high enough to be seen from the ground. The roofing material must have texture and color to complement the material of the body of the building.

## 10.1.8 Terraces and Green Roofs

Terraces created at the stepbacks must allow landscaping for outdoor use of adjacent units. Pergolas are highly recommended along with facilities for drip irrigation. Terraces must be used for outdoor use (dining, relaxation) and not storage.

Green Roofs are defined in *Section 6.0*. Green roofs are recommended on all flat roofs that can be seen from housing units or offices for individual and collective use of residents of the building. Green roofs are optional on the upper roof level. Green roofs provide reduced costs for cooling, increased insulation, less runoff, reduction of urban heat, and enhancement of urban living. These roofs shall be extensive (light shrubbery not to be walked on) or intensive (landscaping that can be walked and played on). It is highly recommended that the roof of the parking deck (building

#2) be intensively landscaped to improve the opportunity for recreation and overall quality of life for the residents of buildings #2 and #3. It is further recommended that, considering the size of the roof, tennis courts and other recreational uses be incorporated into this roof. See **Exhibit 19.0** for recommended locations and access points.

## 10.1.9 Solar

Solar collectors for the generation of electricity and or hot water are highly recommended on the roof tops. The collectors must be integrated into the architectural character of the building and must complement the overall design.

## 10.1.10 Ground Level Building Edges / Frontage Type

Building edges / frontages refer to the uses and architectural character of the first floor relationship to the ground plane. Certain building edges are adjacent to specific pedestrian realms, whereas others have adjacent green areas. See **Exhibits 17.1-17.9**.

In this plan there are nine (9) types of building edges / frontages that have adjacent pedestrian realms:

1. Shop front and Awning at grade (*Exhibit 17.1*)
2. Shop front and Awning on elevated sidewalk (*Exhibit 17.2*)
3. Elevated office frontage (*Exhibit 17.3*)
4. Elevated shop front and awning plaza at tower (*Exhibit 17.4*)
5. Shop front galleria (*Exhibit 17.5*)
6. Transit plaza with shop front and awning (*Exhibit 17.6*)
7. Parking structure and rail line (*Exhibit 17.7*)
8. Parking structure and riverwalk pathway (*Exhibit 17.8*)
9. Tower with stair and riverwalk pathway (*Exhibit 17.9*)

## 10.1.11 Galleria

A galleria is required to provide access to train station plaza from the street (shown in **Exhibit 17.5**). A galleria is a covered area for pedestrians with a consistent rhythm of columns supporting buildings above. The galleria is required to have the following characteristics:

- A. Gallerias shall be double height ranging from a minimum of 23 to a maximum 34 feet in height.

B. All retail fronts along the galleria shall provide a minimum of 80% glass. Storefront windows must provide maximum exposure to window displays, the inside of restaurants, or to other types of ground floor uses.

C. Columns and beams along the galleria shall have architectural features such as arches, plinths, and other visual features.

D. The architecture of the galleria can imitate historic designs but have features and surface patterns that visually reinforce the overall architectural design of the building.

E. The paving along the galleria must be visually distinct and continuous with the plaza. Brick, stone, marble, or other decorative and durable paving materials must be used.

F. If stairs are required due to the change in grade, no more than 3 steps may occur at a time, and a handicapped-access ramp must be provided.

### 10.1.12 Retail Frontages

Retail frontage is required in the locations specified in **Exhibits 8.1 and Exhibits 17.1-17.9**.

1. Entrances must be located at sidewalk grade level.
2. Retractable awnings shall be permitted to a minimum distance of 4 feet to a maximum distance of 8 feet from the building façade.
3. All retail fronts shall provide a minimum of 80% glass area. Storefront windows must provide maximum exposure to window displays, the inside of restaurants, or to other types of ground floor uses. To the extent possible, glass should start at grade or six inches above grade.
4. Shop fronts: The front of each store should be designed individually, with input from its tenant, rather than according to a repeated template.
5. Signing must meet standards set forth in Sections 10.4 and 10.5.
6. Further regulations are specified in **Exhibits 17.1-17.9**.

### 10.1.13 Residential Frontages

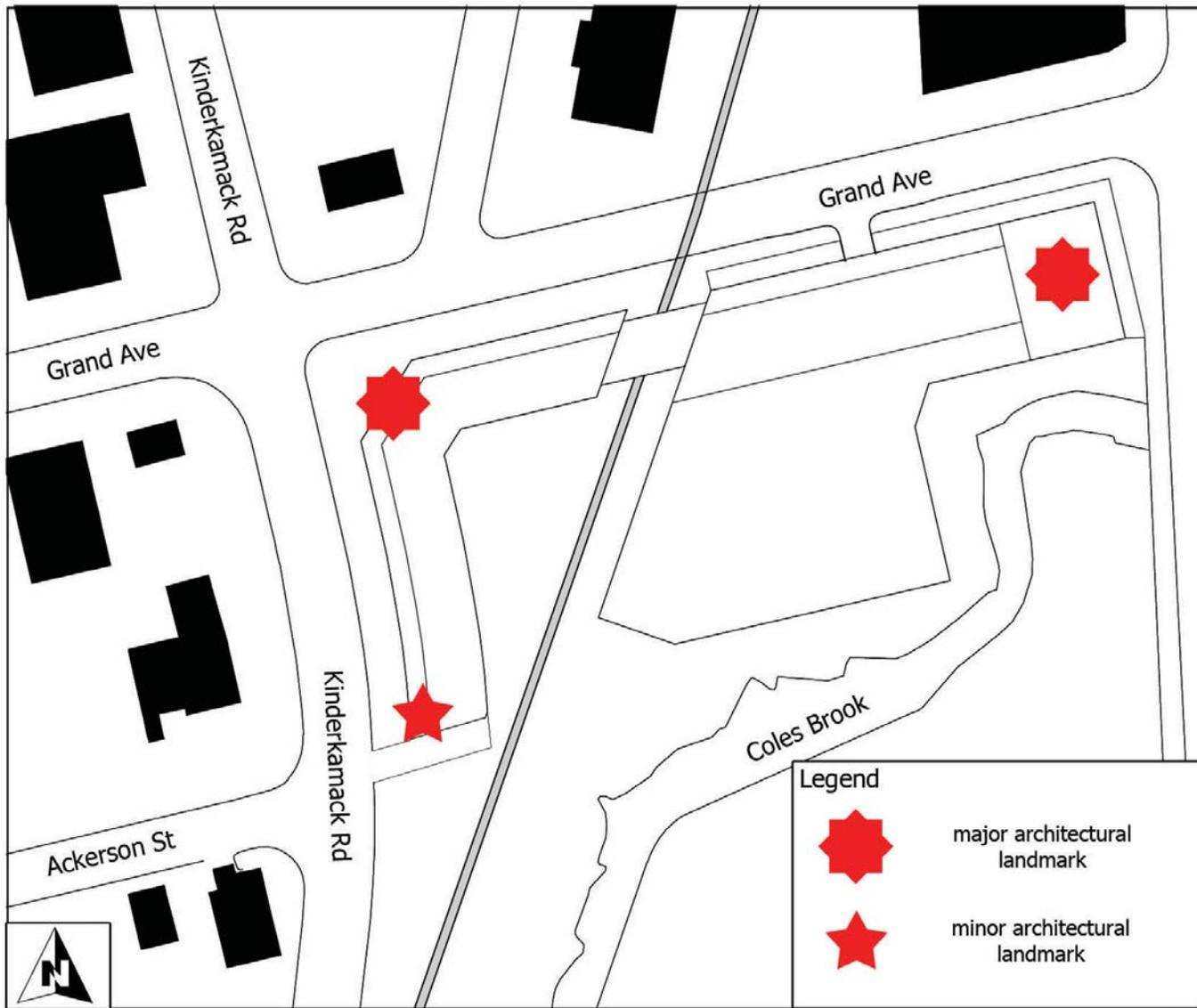
This section applies to those areas at the ground floor that provide access to residential on the upper floors. Recommended locations for residential entrances are shown on **Exhibit 9**. A large lobby must be provided for the residential Building #3 on the corner of Grand Avenue and Hackensack Avenue. Lobbies providing access to multiple units shall have access directly from sidewalk grade without steps, except where steps are needed to comply with flood levels. See **Exhibit 8.2**. Interior access to elevators must comply with ADA requirements. A canvas marquee can be used to emphasize the entrances. Marquees can extend to the curb edge.

### 10.1.14 Office Frontages

1. Offices entrances/lobbies can be located on frontages specified in Exhibit 17.3.
2. Offices are allowed on the first floor provided that a 4 foot semi-public landscaped edge is provided where windows overlook the street.

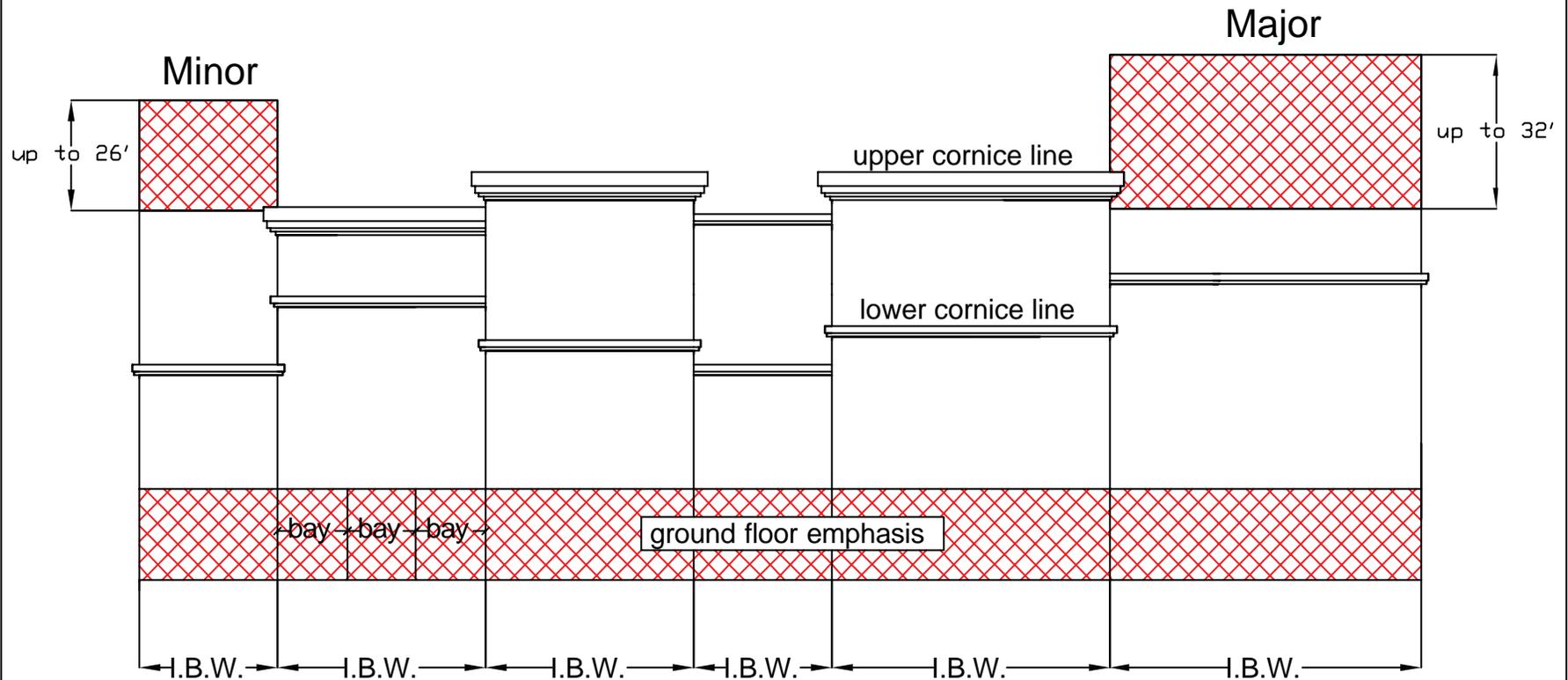
### 10.1.15 Parking Structures

1. Ingress/egress for the structure will interact with the pedestrian realm and shall be designed in such a way to maximize pedestrian safety, and minimize the pedestrian flow and disruption.
2. The exposed facades of parking structures shall either mimic residential façades by using vertically oriented openings that emulate residential window typologies or shall incorporate a metal lattice/screen on which vine type plants can grow. This second alternative is highly recommended along the Coles Brook edge of the parking structure. The more residential facade is required on the facade that edges the railroad and forms the building edge of the station plaza. See **Appendix 2.0** for acceptable examples.
3. Liner buildings are required along the Grand Avenue frontage of the parking structure and shall incorporate a mixed-use residential facade in form and character.
4. Decorative gates and fenestration must complement the architecture of the building facade.
5. Openings in the facades shall be designed to eliminate headlight lamp



Upper and lower cornice line must reflect I.B.W. spacing modules, which must range from 15' to 50'. Each I.B.W must be further subdivided with bays.

Major and minor architectural landmarks may extend up to 32' and 26' (respectively) above the upper cornice line.

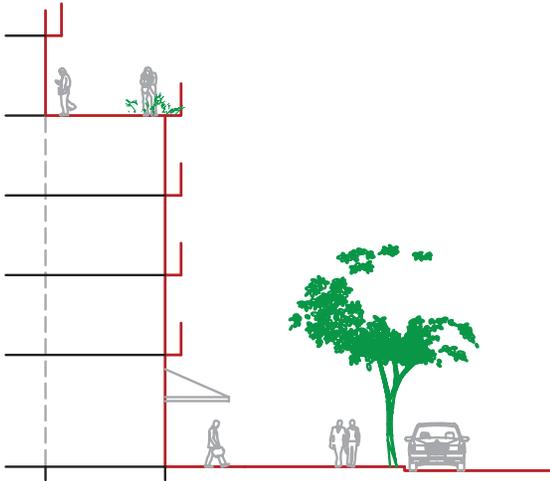


Note: this regulation does not apply to Building #3

# Exhibit 17 Building Frontage Diagrams

Exhibit 17.1

Shop front and Awning at grade



Shop front and awning at grade features ground floor retail with awnings to protect pedestrians from rain and sun.

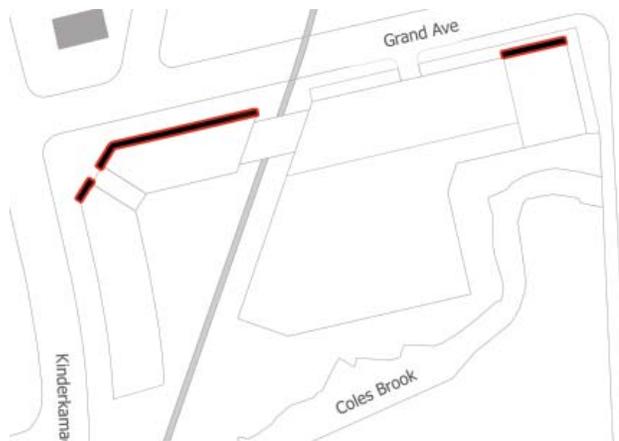
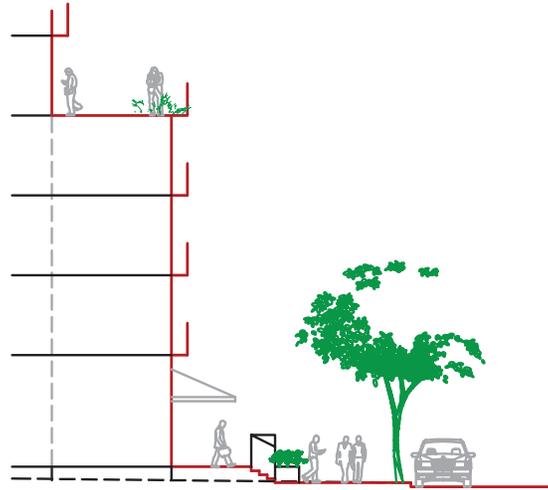


Exhibit 17.2

Shop front and Awning on elevated sidewalk



Shop front and awning on elevated sidewalk receives similar treatment as Exhibit 17.1 with the exception that due to grade change and 100yr floodplain elevation (See Section 9.2), the retail edge sidewalk must be elevated, connected to the lower (at grade) sidewalk by stairs and planters.

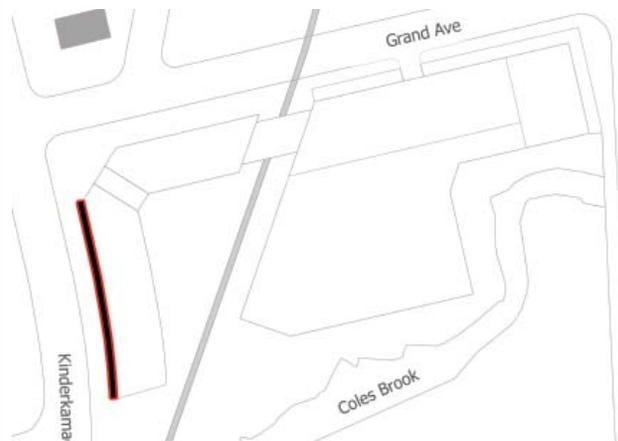
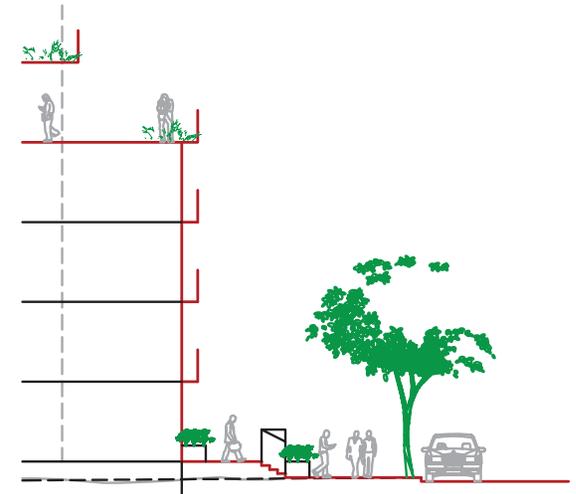
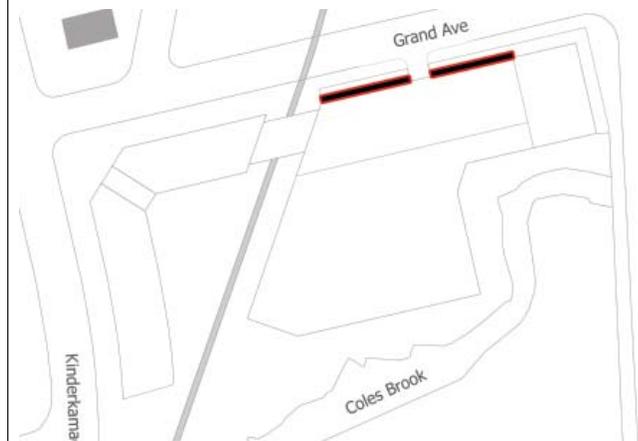


Exhibit 17.3

Elevated office frontage



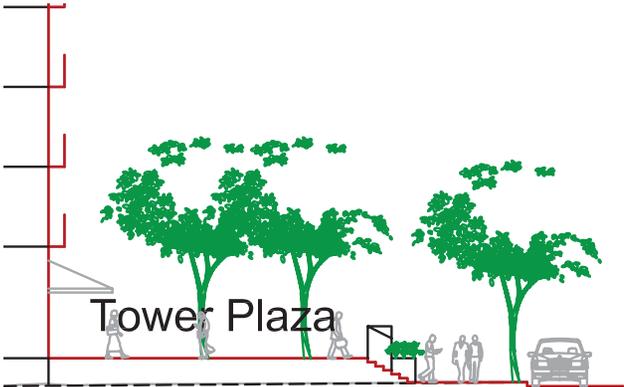
Elevated office frontage also receives the elevated sidewalk treatment. Since the ground floor use is office, there is no awning. Instead there shall be a 2 foot to 5 foot planter at its edge.



# Exhibit 17 Building Frontage Diagrams (Cont'd)

Exhibit 17.4

Elevated shop front and awning plaza at tower



Along Hackensack Ave, there shall be an elevated plaza (according to Section 9.2) at the edge of the tower (building #3). This plaza shall be hardscaped, with plantings and trees pursuant to Appendix !.0.

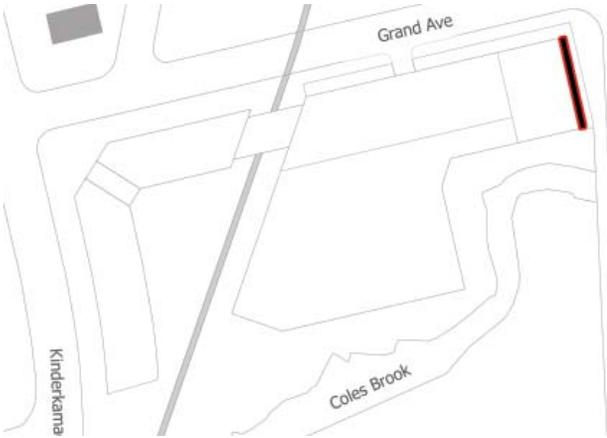
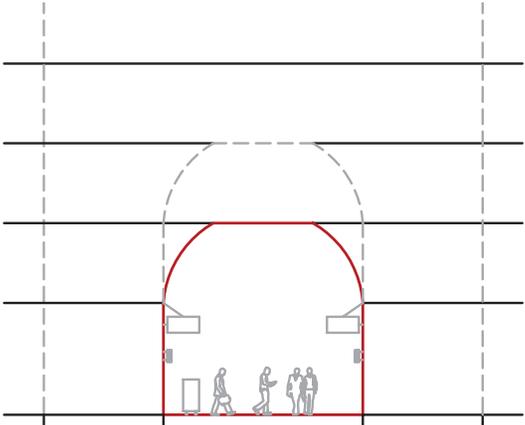


Exhibit 17.5

Shop front gallery



The gallery connects the pedestrian corner of Kinderkamack Rd and Grand Ave to the interior transit plaza. The retail uses that occupy the first floor in these locations should have display windows and an entrance along this edge. The gallery may be from 23-34 feet in height, although uses other than retail may not front onto the gallery.

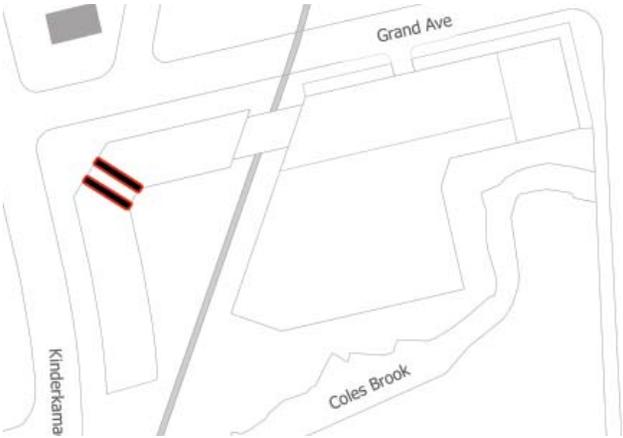
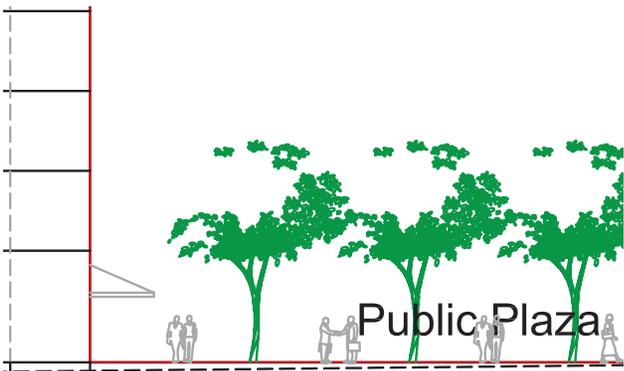


Exhibit 17.6 Transit plaza with shop front and awning



The transit plaza will have retail at the ground floor. This retail frontage shall have an awning.

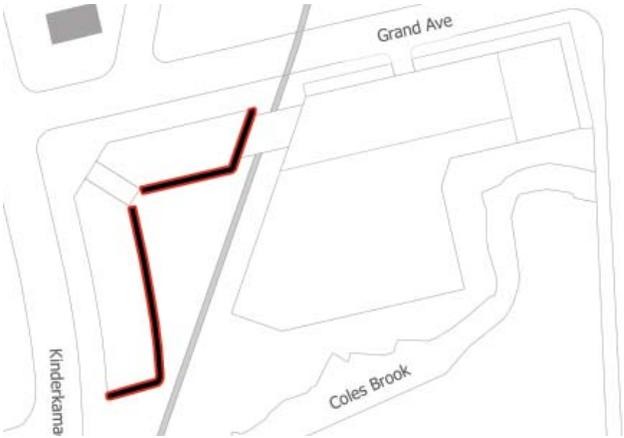
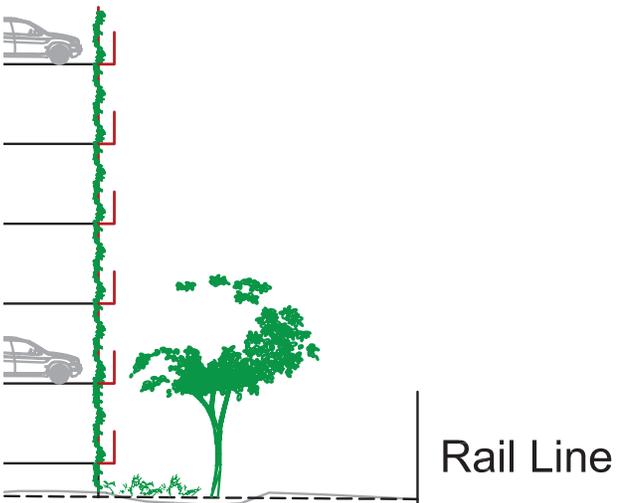


Exhibit 17 Building Frontage Diagrams (Cont'd)

Exhibit 17.7 Parking structure and rail line



Where the parking deck fronts onto the rail line, the edge shall be heavily landscaped. Fenestration in the parking deck walls shall have openings that emulate residential buildings in the Area.

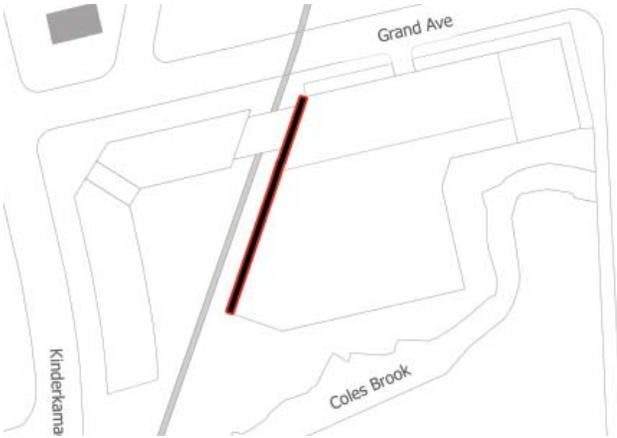
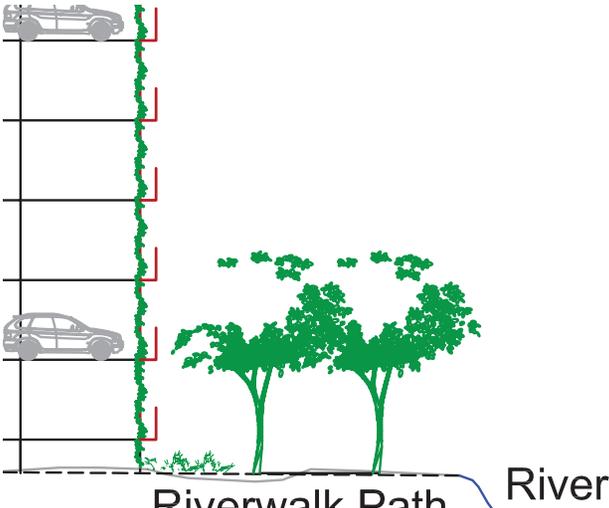


Exhibit 17.8 Parking structure and riverwalk pathway



Where the parking deck fronts onto the Cole's Brook, the edge shall be heavily landscaped. A riverwalk Path could run between a double row of trees. The wall of the parking deck shall be green faced, or screened with vegetation (see Appendix 2.0).

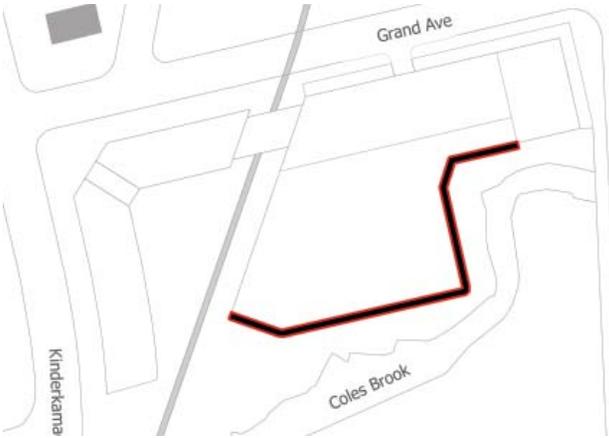
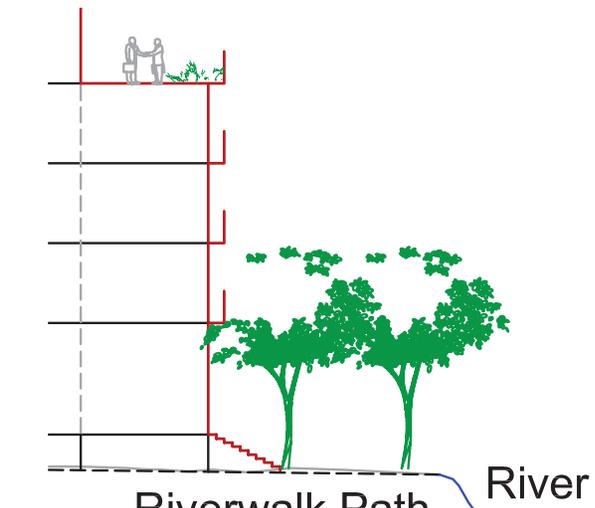
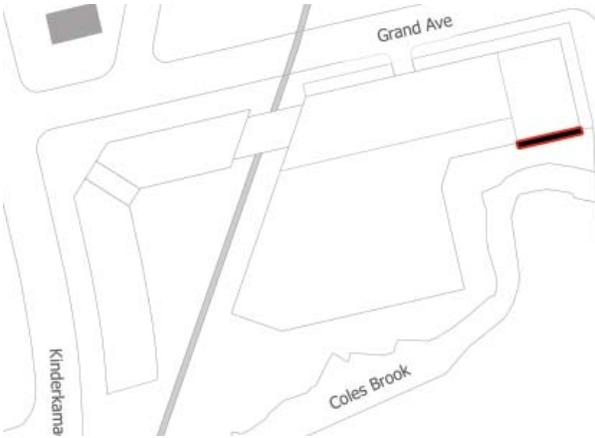


Exhibit 17.9 Tower with stair and riverwalk pathway



Where the Tower (building #3) fronts onto the Cole's Brook, there could be stairs leading down to the recreational path.



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glare from the inside to the outside of the structure.

6. Standards for parking decks are further illustrated in **Exhibit 14.2 and 14.3.**

### 10.2 ADDITIONAL ARCHITECTURAL STANDARDS

The standards previously stated in this Redevelopment Plan and the following required standards will insure that the proposed buildings are compatible with the positive images from the River Edge Town Center Visual Preference Survey and specifically illustrated in the Design Vocabulary while insuring the proposed buildings are “of its time.”

A. Buildings shall be designed with a horizontal subdivision that emphasizes the base, middle, and upper stories/cornice. Buildings shall have a vertical, not horizontal emphasis by use of IBW.

B. Buildings shall be designed to be aesthetically pleasing from all viewing points, using similar materials and fenestration on all facades except for the facade along Coles Brook.

C. Façade Materials: The primary facade materials shall be stone, masonry, brick, real stucco, glass and metal. No synthetic materials are allowed. Facades on Individual Building Widths (IBW) should consist of no more than three basic materials with a variety of textures and accents. A range of colors can be used to provide emphasis on the verticality of the individual building widths.

D. Façade Colors: A combination of lighter colored masonry/precast concrete and complementary darker brick in the earth tones shall be used at the lower facade (below first setback).

E. Variety: Buildings, particularly longer buildings fronting along Grand Avenue, should be designed in individual building widths expressed in different exterior materials or colors, roof treatment, cornice treatment, setbacks that create the appearance of having several smaller buildings arranged next to each other when viewed from the street. Each building within the redevelopment area can have a variation in architectural

expression thereby preventing the “institutional” look.

F. Each individual building width or combination of these along a streetwall must be expressed vertically from the ground level to the skyline, enhanced with decorative cornices at the setbacks and at the skyline. See **Exhibit 16.0.**

G. Rooftop mechanical equipment shall be screened in order to minimize the negative aesthetic impact upon the viewer both from street level and as may be viewed from surrounding buildings. Screening shall be consistent with the architectural design and materials of the building and may include higher parapets.

H. Mechanical structures shall be fully integrated with the architectural and structural design of the building.

I. All parts and components of cellular phone antennas, satellite dishes, and television and radio antennas and solar collectors shall be designed to be in harmony with the architectural context. Screening shall only be required in such cases where integration with the architecture of the building is not aesthetically responsible.

J. Building Frontages / Individual Building Width Spacing: All buildings are required to form an articulated building wall. Grand Avenue and Kinderkamack Road facades should be divided into individual building widths ranging from a minimum of 15 feet to maximum of 50 feet on center. Individual building widths must be expressed in changes in masonry color or pattern, modulation of the windows, variation in cornice design, setback between individual building widths etc. See **Exhibit 16.0.**

K. Building attachments: Awnings, projecting windows, roof overhangs, french balconies may encroach in front of the build-to-line, provided they conform to the standards set forth in the Encroachment Standards - See **Exhibit 14.** The maximum dimensions are listed below. Inclusion of any building attachment above those specified as encroachments shall be subject to the approval of the Planning Board. The maximum dimensions that may be so approved are as follows:

a. Awnings on the ground floor may extend to a maximum of 8 feet from the building facade and may not place supports upon the

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public sidewalk except if it is a marquee.

b. Bay Windows may extend to a maximum of 3 feet from the building façade.

c. Projecting/cantilevered “french balconies” may extend to a maximum of 1 foot from the building façade.

d. Architectural expressions on street corners may project up to 4 feet past the build-to-line.

L. Story Heights: See **Section 9.3.4**.

M. All retail establishments within a building shall place their primary entrances at sidewalk grade and have their doors recessed so as not to swing out onto the public sidewalk.

N. Prominent Entries: Main building entries shall be at grade, or on the elevated sidewalk where necessary to be above the flood level, see **Exhibit 8.2** and be easily identifiable as such with prominent architectural features; they should not occur simply as voids between buildings. All buildings shall have entrances accessed directly from the public sidewalk. Building 3, the “tower” shall have a prominent entry featuring a large main lobby and entry to prominent retail elements.

O. Façade Ratio: The percentage of void area (windows and other openings) in a building façade shall be between 40% and 60% at the lower facade and between 60% and 95% at the upper facade. Street-level retail frontages shall not be less than 80%.

P. Façade Composition: “Scattered-window” facades shall not be allowed along any frontage facing a public right-of-way. Each such façade shall present a unified, rational composition.

Q. Façade Materials at Corners: At predominant building corners, the façade materials must be the same on both faces meeting at the corner. These materials must extend at least 15 feet from the corner on both faces.

R. A major landmark corner shall be located on the tower (building #3) at the intersection of Grand Avenue and Hackensack Avenue, as well as a major

landmark at the intersection of Grand Avenue and Kinderkamack Road (building #1) and a minor landmark at the termination point of building #1 (near the intersection of Kinderkamack Rd and Ackerson St). Corners have been specified as major or minor landmarks (**see Exhibit 15.0**). There must be a clear difference between each type. Such expression can include projections, towers, roof forms, height increases or other architectural appurtenances appropriate to the scale and architectural expression and style selected. Stepbacks may be eliminated at the corners. Major landmarks are located on the primary corners at the intersection of primary streets. The corner of Grand Avenue and Hackensack Avenue is the location that shall have a major architectural embellishment signaling the entry into the area. Minor landmarks are recommended on secondary corners along major streets.

S. Parking Entrances: Parking structure entrances shall not simply be gaps between buildings, but through vehicular openings in the frontage line wall of the building. The parking structures shall provide direct pedestrian access to sidewalks such that users may exit the parking facility without entering a building. All vertical circulation areas within a parking facility shall be lit in the daylight-incandescent range. Pedestrian entrance doors shall be of a complimentary architectural style as the building and be painted a complementary color of the building.

T. Interiors of parking garages shall be painted white or a light color with each floor clearly identified.

U. Antennas: Antennas of any type shall only be permitted on the exterior of any building with the approval of the Planning Board as to their size, shape and location. If allowed they must be properly screened.

V. Utilities: All machinery and the mechanical controls for same, including but not limited to transformers, junction boxes, lift stations, electrical meters, condensers, and signal boxes, shall be interior to the block and masked from frontages by building elements in a manner consistent with the design of the building, incorporating false windows and dispersed venting to maintain the window rhythm and building pattern design. A wall of venting for mechanical rooms shall not be permitted.

W. All buildings shall display the street address of the building and/or name

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of building such that it is clearly visible from the adjoining street right-of-way day and night.

X. Flat roofed buildings shall have parapets that vary in height by the bay module and have unique decorative cornice.

Y. Terraces at the stepback levels shall be extensively landscaped with paving materials, planters, pergolas etc.

Z. Windows above the first level should have a vertical orientation. Windows shall be framed using sill and lintel. Punched windows with no decorative edging are not allowed. Window frames should be colored. White window frames are not allowed. Strip windows should not be used; i.e., no window should have a width to height ratio of greater than 2:1 without a separation from the adjacent window formed by the main building façade material. This requirement should neither apply to windows used for ground floor retail purposes, nor to the top floor of any building or clerestories. Windows must be set back a minimum of 3 inches from the primary facade.

AA. Night security gates, grids, or any other security covering of windows is prohibited.

### 10.3 OPEN SPACE AND LANDSCAPE PLAN DESIGN REQUIREMENTS

The street landscaping improvements identified in this section are dependant upon the execution of a RAD by the Borough of River Edge. The actual construction improvements shall be funded by the RAD, which is described further in the Redevelopment Plan Implementation Section.

The provision of “green areas” to include street trees, ground cover, small parks/plazas, planters, terraces and green roofs are necessary components of in-town living. Street trees must be provided. Bulb-outs with sufficient area for tree planting is recommended along Grand Avenue and Kinderkamack Road. A small transit plaza with a water element is a required behind Buildings 1 and 2, in addition to the streetscape. The recommended and illustrative Landscape Plan is shown on **Exhibits 18.0 and 19.0.**

Prior to the commencement of construction within the redevelopment area, an overall landscape plan for the streets, sidewalks, crosswalks, arcades, planters,

green roofs, terraces and plazas shall be presented and approved by the Borough Engineer for the streetscapes (lights, paving, street trees) and the Planning Board for all other landscaping.

The aforementioned plan shall be prepared by an experienced landscape architect. It shall incorporate the rights of-way design parameters for landscape as specified in this Redevelopment Plan, while providing sufficient detail as to how other open areas in the plan will incorporate quality materials and plant types.

The following additional standards shall apply:

A. The Landscape Plan shall include the highest and best quality decorative design materials available. The Landscape Plan shall identify at minimum architectural pavers, decorative lighting, tree species, and decorative site furnishings, including all specific color and material selections.

B. Street trees and plaza trees must be a minimum of 4 inches in caliper and 16 feet high at the time of planting. All street trees types and sizes shall be recommended by a local arborist, nurseryman, or some other landscape architect designated by the Planning Board.

C. Street trees shall be planted in adequately sized tree wells, with appropriate watering and feeding techniques. The area around the tree can be planted with groundcover, covered with decorative grates, or paved with decorative pavers.

D. All landscape materials must have a two year maintenance guarantee.

E. All ground areas that are not paved shall be landscaped with shade trees, flowering trees, evergreen and deciduous shrubs, and perennials and bulbs. Only plant materials with proven resistance to the urban environment should be utilized. Such lists are created by the Community Forestry Council, a division of the New Jersey Division of Parks & Forestry, or the New Jersey Nursery & Landscape Association.

F. All plantings should be done following current horticultural practices for urban locations. A recommended list of those practices is set forth in Appendix 1.



Exhibit 19.0 Recommended Green Roof Location



## 10.4 STREETScape REQUIREMENTS

The standards set forth in this section shall be binding for streetscape development. The exact configuration to be utilized within each right-of-way shall be made by the Borough Engineer, subject to the approval of the Redevelopment Entity.

### A. Streetscape Materials:

- a. Vehicular cartpaths shall be constructed of asphalt.
- b. All curbing shall be granite curbing or poured concrete colored French Grey and brushed. Asphalt curbing is expressly forbidden. Curbs shall be 6" height without horizontal lips.
- c. Sidewalks shall be constructed of textured paving materials or concrete with brick edges and dividers.
- d. Crosswalks shall be of similar material and color as the paved sidewalks.
- e. No more than three paving materials or colors should be used per block.
- f. The surface plaza and transit plaza shall be a combination of brick and field stone. Modular paving or stamped concrete are also acceptable pursuant to Planning Board review. **Appendix 2.0** illustrates acceptable examples.

### B. Street Tree Planting: The street trees shall be regularly spaced in accordance with the specifications in **Exhibits 18.0**.

- a. Street trees shall be planted in tree wells with decorative grills. Where possible, tree wells shall be protected by a low fence.
- b. Street trees shall be a minimum of 4" caliper 16 feet high at the time of planting.
- c. Selection of tree types shall be made by the Planning Board and the local arborists.

### C. Street Lighting: Street lights shall be placed at the outer edge of all sidewalks and shall be as follows:

- a. A single lighting standard design for posts and fixtures must be used throughout the redevelopment area, with the mass and size varying by location and street type. The fixture type can be modified for use outside or inside of the Galleria. Final approval of the fixture, pole type, and location will be made by the Planning Board.

b. Street Frontages: One lighting standard, not to exceed 15 feet in height, for every 25 linear feet of sidewalk, on average.

c. Corners: Lighting must be increased in height and coverage on corners. Lighting standards should be placed on all four corners of intersections in excess of two lanes

d. All lighting shall be shielded to minimize night sky and prevent direct glare into adjacent residential windows.

e. Where possible, light levels shall be controlled to reduce lighting levels to the lowest possible standard that still permits safe passage at such hours of the night and at such locations that have little or no pedestrian activity.

f. Adjustments to these requirements may be made relative to one another in response to the photometric specifications of the chosen light standards. Street lights shall be placed by beginning at corners (without blocking crosswalks) and then working inward towards the middle of the block. Streetlights should produce a spectrum in the daylight-incandescent range.

g. Key lighting of predominant architectural details is highly encouraged, as well as display lighting in display windows.\

i. the Riverwalk Path shall be uplit.

### D. Street Signage: Wherever possible, public signage shall be consolidated and affixed to lampposts.

E. Obstructions: Trash receptacles, mailboxes, bicycle racks, shall be located at the outer edge of the sidewalk. Exceptions: Vending racks and sidewalk dining may encroach upon a portion of the sidewalk providing that a 4 foot clear aisle is maintained, not including the area reserved for street tree planting. Benches shall be placed against building walls or at street edge facing retail fronts.

F. Streets: The outer 3 feet of sidewalks shall be constructed of bricks or similar approved pavers set between flush mounted planting wells along Grand Avenue, Kinderkamack Avenue and Hackensack Avenue. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.

G. All other Borough of River Edge streetscape design standards, as provided in the Borough Code shall be adhered to.

## 10.5 SIGNAGE REQUIREMENTS

In addition to adhering to the general standards contained within the Borough of River Edge Code, the following signage requirements apply to all uses:

- A. All signage shall be subject to Site Plan review and approval by the Planning Board.
- B. No fluorescent or glowing paint is permitted for any signage within the Area.
- C. No signs or advertising devices that are rooftop mounted, intermittently illuminated, flashing, or moving are allowed. Signs that might be mistaken for traffic control devices are also prohibited.
- D. Information kiosks with tenant listings and directions are permitted with a maximum of 8 square feet of signage area. Advertising is prohibited on kiosks.
- E. Freestanding signs are prohibited with the exception of way-finding identification installed with the approval of the Borough of River Edge.
- G. Signage above the second story of any structure is prohibited.
- H. Residential: A lobby serving more than 20 units may have a sign naming the building which may not exceed twenty square feet. The style of the lettering must be in character with the architecture of the building. Signage at entrances to individual units is limited to a street number not to exceed one square foot.
- I. Retail: Every retail establishment is permitted one facade sign and one sign per sidewalk frontage. Façade signage on windows or above display windows shall not exceed 30 inches in height. Blade signage shall not exceed 18 inches in height, nor project more than 3 feet from the façade. One hanging sign not to exceed four square feet is allowed for each retail use under the arcade.
- J. To the extent possible, lighting levels for signage should be controllable allowing the level of intensity to be significantly reduced at times of low pedestrian or vehicular activity.

K. Window displays for goods and services provided by retail establishments are encouraged and shall be lit with appropriate display lighting throughout the entire night.

L. Other sign requirements based on the existing ordinance that are not in direct conflict with the standards set forth above.

# VALIDITY OF THE PLAN

## SECTION 11.0 VALIDITY OF THE PLAN

If any section, subsection, paragraph, division, subdivision, clause or provision of this Plan shall be deemed by the courts to be invalid, such adjudication shall only apply to the particular section, subsection, paragraph, division, subdivision, clause or provision in question, and the balance of the Plan shall be adjudged valid and effective.

# OTHER PROVISIONS

## SECTION 12.0 OTHER PROVISIONS

### 12.1 ZONING MAP REVISIONS

The Zoning Map of the Borough of River Edge is hereby amended and shall be revised to show boundaries of the “Area” defined in this plan and identify the district as “The New Bridge Landing Station Redevelopment Area” and all provisions of this plan shall apply.

### 12.2 PROJECT SIGNS AND ADVERTISING

In order to facilitate the overall redevelopment of The New Bridge Landing Station Redevelopment Area, and to encourage further investment in the surrounding area and the Borough of River Edge in general, all advertising, signage, and other promotion of the development and redevelopment of the “Area” shall contain references to the proposed project’s location in the Borough of River Edge, so as to promote the positive aspects of the project, the New Bridge Landing Station Redevelopment Area, and the Borough of River Edge.

### 12.3 AFFORDABLE HOUSING

All development within the redevelopment area must conform with the latest Council on Affordable Housing (COAH) Standards for the number, size and cost of affordable units. The need for affordable housing based on number of proposed units and new retail/commercial must be accommodated within each focus area or by agreement with the developer within the Redevelopment Area.

### 12.4 ENVIRONMENTAL REGULATIONS

All development within the redevelopment area must conform to the latest EPA and NJDEP environmental standards as of December 6, 2006.

# PLAN CONSISTENCY REVIEW

As required by the Redevelopment and Housing Law, this section describes the consistency between the New Bridge Landing Station Redevelopment Plan and the Borough of River Edge's Master Plan and how the provisions herein are designed to effectuate the Master Plan. This Plan also describes the relationship of the New Bridge Landing Station Redevelopment Plan to the master plans of contiguous municipalities; the Bergen County Master Plan, and the New Jersey State Development and Redevelopment Plan.

### 13.1 RELATIONSHIP TO LOCAL OBJECTIVES OUTLINED IN THE BOROUGH OF RIVER EDGE MASTER PLAN (2003)

An analysis of the 1984 Borough of River Edge Master Plan and its 2003 Re-Examination Report did not reveal substantial inconsistencies with the goals and objectives of the New Bridge Landing Station Redevelopment Plan. The Re-Examination—chiefly, its recommendation for major mixed-use redevelopment of the southern sector of the municipality—lends support to Plan.

The New Bridge Landing Station Redevelopment Plan addresses many of the goals and objectives articulated in the 1984 Master Plan, including:

- Consideration of “the redevelopment of the area of town south of Main Street with a mixture of commercial and residential development.”
- Exploration of “development in and around the Train Station in the south end of town to exploit the available public transportation and nearby highways...”

Additionally, the “Recommended Changes” section of the 2003 Re-Examination Report lends support to the Redevelopment Plan:

- “[B]etter utilize the commercial district, specifically the area from Main Street south to the Hackensack border as many of the lots in this area are underutilized and do not constitute the highest and best use given the location.”
- “[E]xploit the proximity of the New Jersey Transit train station and develop mixed use development in the area.”
- “[A]ssure the highest and best and development of the area” by reconsidering building height and density requirements and re-evaluating permitted zoning uses.

The New Bridge Landing Station Redevelopment Plan, which creates a pedestrian-oriented, mixed-use streetscape, would help achieve the objectives of the Master Plan and its Re-Examination regarding the exploitation of regional transit and the appropriate mix of land uses in the area.

### 13.2 RELATIONSHIP WITH EXISTING ZONING ORDINANCE

Based on the Land Use Map in the 1984 Master Plan, the land within the Redevelopment Area is composed of three uses. The parcels west of the rail ROW is a surface parking lot serving the station, described as Public & Semi-Public use. Just east of the rail is a strip of vacant, wooded land. The eastern portion of the Area is a vacant commercial lot that is the site of a demolished car wash.

The Redevelopment Area falls entirely within the C2 Commercial zone, which includes business and professional offices, public buildings and uses, funeral parlors, banks and financial institutions, department stores and supermarkets, real estate and insurance offices, restaurants and churches and schools. The zoning code described in this Redevelopment Plan supersedes the zoning ordinance of the Borough.

### 13.3 EFFECT ON ADJOINING MUNICIPALITIES

#### Hackensack

The Redevelopment Area lies in southern River Edge, partially on the border of Hackensack. The parcels directly adjacent to the Area are office towers fronting Hackensack Avenue. These towers have large surface and structured parking lots that abut the Area. Route 4, Hackensack Avenue, and their associated interchanges create a large buffer around this area. On the eastern side of Hackensack Boulevard (across from the Redevelopment Area) are large industrial complexes with mass amounts of surface parking. The scale and use proposed by the Redevelopment Plan are not in conflict with those on these parcels. However, density and value added to the Redevelopment Area may spur consideration for redevelopment on these parcels as well.

#### Paramus

The southeastern edge of the Borough of Paramus comes within approximately one-quarter mile of the western border of the redevelopment area. Local connection is

## SECTION 13.0 PLAN CONSISTENCY REVIEW

provided via Grand Avenue to NJ Route 4. Route 4 entering Paramus is lined with commercial and office uses. The Paramus Master Plan considers uses along both the border in both municipalities to be compatible, and makes no mention of River Edge redevelopment efforts. It is expected that residents of Paramus may take advantage of the improved train station area and parking facilities easily accessed from Route 4.

### **New Milford**

New Milford is approximately one-third mile from the Area across the Hackensack River. The municipality is accessed most directly by Hackensack Avenue, which becomes New Bridge Road as it crosses the river. The transition from commercial/industrial uses to single family is buffered not only by the river and bridge but also by a stretch of tree lined road leading to and from the bridge on the River Edge side. No conflict between the Redevelopment plan and this municipality are anticipated.

### **Oradell**

River Edge shares its northern border with the Borough of Oradell, over a mile north of the Redevelopment Area. The most direct route between the Area and Oradell is Kinderkamack Road. Because of the distance, there is no anticipated conflict or relationship between the Area and the Borough of Oradell.

### **13.4 CONSISTENCY WITH THE BERGEN COUNTY MASTER PLAN**

The Bergen County Master Plan is currently in the process of being written. When it is completed, this Plan is subject to consistency review.

### **13.5 CONSISTENCY WITH THE NEW JERSEY STATE DEVELOPMENT AND REDEVELOPMENT PLAN (2001)**

The New Bridge Landing Station Redevelopment Plan furthers many of the goals and objectives outlined in the New Jersey State Development and Redevelopment Plan. As a part of the Metropolitan Planning Area (PA1), the Redevelopment Plan is consistent with the State Plan's intent to promote growth in compact form. Because this area includes the train station, intensities of residential and commercial uses are expected to be sufficient to support transit, promoting pedestrianism and reducing dependence on the automobile.

### **13.6 CONSISTENCY WITH SMART GROWTH PRINCIPLES**

The State of New Jersey Office of Smart Growth has promulgated principles of Smart Growth to which redevelopment plans must conform. The New Bridge Landing Station Redevelopment Plan is in maximum conformance with these principles. It is mixed use and compact, in a walkable neighborhood, offering a distinctive and attractive sense of place. It uses existing infrastructure and promotes transit use. It has reached out to the community in the conceptual design process.

# ACQUISITION AND RELOCATION

## SECTION 14.0 ACQUISITION AND RELOCATION

Since the properties concerned with this plan are owned by either the Borough or NJTransit, no acquisition or relocation is required. However, surface parking will be displaced. Therefore, it is suggested that Parcel B be built first so that the existing parking lot may continue to be used while the parking structure is constructed. Once the parking deck is constructed the surface parking can be relocated there, and Parcel A will be able to be built. An interim commuter parking plan must be developed and submitted to NJ Transit for approval prior to commencement of any phase of development that would result in relocation or reduction in existing NJ Transit commuter parking.

# REDEVELOPMENT PLAN IMPLEMENTATION

## SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

### 15.1 REDEVELOPMENT ENTITY

The Redevelopment Entity shall have such powers and duties as are set forth in New Jersey's Local Redevelopment and Housing Law (LRHL) and as may be conferred by this Redevelopment Plan, including but not limited to the authority to acquire real property, to relocate residents and businesses, to designate redevelopers, to establish clear terms and conditions for redevelopment through the negotiation, execution and administration of Redeveloper's Agreements, and to do such other things as provided by law.

#### 15.1.1 Redevelopment Plan Requirements

The Redevelopment Entity will review concept plans provided by the redeveloper before submission to the Borough Planning Board. This process will help assure the Borough's redevelopment entity that the redeveloper is following the design standards appropriately.

### 15.2 PARCELIZATION

The diagrams, images and other graphic representations provided in this Redevelopment Plan are intended to provide a framework for interpretation of the written standards and regulations contained herein. Nothing in this Redevelopment Plan shall preclude the partial redevelopment of a block depicted in such diagrams, images or other graphic representations, provided that such subdivision or re-subdivision and partial redevelopment of a block is fully in conformance with the written standards and regulations contained herein. (Such parcelization of any disposition parcel shall be subject to approval by the Planning Board and shall be reflected within the Redeveloper Agreements as may be executed between the Redevelopment Entity and such Redevelopers as may be designated by the Redevelopment Entity.)

### 15.3 PLANNING BOARD REVIEW AND APPROVAL

A. The Planning Board shall have the right to review and approve a redeveloper's plans and specifications with respect to their conformance to this Redevelopment Plan. Such a review shall be on the basis of a site plan submitted to the Planning Board. No additional construction or alteration to existing or proposed constructions shall take place until a site plan reflecting such additional or revised construction has been submitted to and approved by the Planning Board. This pertains to revisions or additions prior to, during and after completion of the improvements.

B. Prior to commencement of: (a) any new construction, (b) reconstruction, (c)

rehabilitation, (d) any change in the use of any structure or parcel, or (e) any change in the intensity of use of any structure or parcel, a site plan for such shall be submitted by the redeveloper or property owner to the Planning Board for review and site plan approval. No building permit shall be issued for any work associated with items (a) through (e) above without prior site plan review and approval by the Planning Board.

Site plan review shall be conducted by the Borough of River Edge Planning Board pursuant to NJSA 40:55D-1 et seq. Site plan review shall consist of a preliminary site plan application and a final site plan application which may be heard concurrently. Submission of a site plan and site plan application shall conform to the requirements of this Plan. Applications may be submitted for individual buildings or groups of buildings, entire blocks or portions of a block, or a project group. Final site plan approval for any construction shall not be granted unless or until that construction is substantially complete, or performance guarantees for site improvements for that phase have been furnished by the redeveloper in accordance with NJSA 40:55D-53.

No Certificate of Occupancy of any type shall be issued for any construction until the Planning Board has given final site plan approval for the building, building group, block, or project group in which such construction is located. As part of the final site plan approval, the Planning Board may require a developer to furnish performance guarantees pursuant to NJSA 40:55D-53. Such performance guarantees shall be in favor of the Borough of River Edge and in a form approved by the Borough of River Edge.

The Planning Board at the time of site plan review and without formal amendment to this plan, may approve minor modifications from the standards set forth herein, if deemed to be in the interest of project implementation and in furtherance of this Plan and the standards set forth herein.

### 15.4 REVENUE ALLOCATION DISTRICT

RECOMMENDATION FOR THIS AREA TO BE INCORPORATED INTO A REVENUE ALLOCATION DISTRICT (RAD)

A. Tax Increment Financing and a RAD: Tax Increment Financing uses one or more of the following:

- 1) The incremental increase in property tax revenues created by

## SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

redevelopment (the “Property Tax Increment”)

- 2) Incremental Payments in lieu of taxes (“PILOTS”) and/or
- 3) Parking revenue from public parking facilities to fund the debt service on municipal bonds that the municipality issues to finance a project, called the “Project”, in the RAD.

B. The planned developments in the New Bridge Landing Station District must be likely to be realized by private enterprise, but would not likely be accomplished by private enterprise without the creation of the RAD and the revenue allocation financing of the proposed Project or Projects.

C. Smart Growth: The creation of the RAD must be in compliance with smart growth planning principles. To this end, the State Planning Commission must provide Initial Plan Endorsement to all RADs pursuant to N.J.A.C. 5:85-7. “Smart Growth” means well-planned, well-managed growth that adds new homes, creates new jobs and promotes redevelopment and urban revitalization, while preserving open space, farmland, and environmental resources as set forth in the State Development and Redevelopment Plan.

D. Property Tax Increment: The Property Tax Increment for the District is calculated pursuant to the formula set forth in the definition of “Property tax increment” in N.J.S.A. 52:27D-461, which is as follows:

[General Tax Rate of the municipality For The Current Tax Year x The Taxable Value Of All Property Assessed Within The District In That Year, excluding any special assessments] x [(Taxable Value of All Property in the District - the aggregate taxable value of all property assessed which is located within the District as of October 1 of the year preceding the year in which the District is authorized) ÷ Taxable Value Of All Property Assessed Within The District] = Property Tax Increment

E. Sufficiency of Pledged Funds: The Property Tax Increment of the RAD, PILOTS from the RAD and/or parking revenue from public parking facilities built as part of the Project must be sufficient to meet the debt service (principal and interest payments) on the Bonds to finance the Project.

F. Public Project for Tax Exempt Bonds: In order for the interest payments on the municipal bonds to be exempt from Federal Income Taxation the Project must be a public project.

G. Municipal Guaranty of the Bonds: While not legally required, in order for the bonds to have a high rating and a good interest rate, the municipality must guaranty their repayment.

- H. Four (4) Step Process to Create a RAD and Issue Bonds Pursuant Thereto:
1. RAD Plan Assessment,
  2. Application for approval to create a Revenue Allocation District and Preliminary Revenue Allocation Plan,
  3. Approval of a Final Revenue Allocation Plan, and
  4. Approval of a financial instrument.

Anticipated redevelopment projects and activities for which the use of RAD revenues anticipated is:

- a. Acquisition of private properties
- b. Construction of new or realigned streets;
- c. Public improvements, inclusive of:
  1. Pedestrian walkways and public plazas;
  2. Lighting, streetscape, and landscape improvements;
  3. Other public improvements determined to be necessary or desirable for the effective implementation of the Redevelopment Plan.

### 15.5 NJ TRANSIT REVIEW AND APPROVAL OF REDEVELOPMENT PLAN IMPLEMENTATION

Developers will be required to negotiate with NJ Transit for the use and/or redevelopment of all NJ Transit owned lands within the redevelopment area, independent of the Borough and subject to NJ Transit Board Approval.

#### 15.5.1 Reimbursement for Borough Costs

A designated developer shall be required to pay all applicable escrow fees and other required charges for municipal professionals in a manner similar to the applicable provisions of the Borough’s Site Plan Ordinance and New Jersey

## SECTION 15.0 REDEVELOPMENT PLAN IMPLEMENTATION

statutes as in the case of Planning Board review. In addition, a redeveloper shall be required to pay its proportional share of the costs of any studies, plans, reports, fiscal impact studies, or analyses prepared by the Borough or its designated redevelopment entity as part of the implementation of the Redevelopment Plan. Any such payments required to reimburse the Borough shall be specified in the Redevelopment Agreement and an interim cost sharing agreement effective prior to the Redevelopment Agreement.

Redevelopers further will be required to reimburse the Borough for all of its costs in the preparation of this Redevelopment Plan in proportion to the intensity of development permitted on the parcel being developed by that Redeveloper.

### 15.6 FINANCIAL INCENTIVES

The Borough of River Edge may use its redevelopment powers pursuant to the LHRL to provide financing for any aspect of the Plan including (1) direct financing secured by revenues from any portion of the project undertaken pursuant to the Plan or by the general taxing power of the Borough; or (2) making of any loans to the designated redeveloper; or (3) the extension of credit to the designated redeveloper in such form as the Borough of River Edge determines is appropriate. The extension of credit shall not include a guarantee by the Borough of River Edge of any debt.

### 15.7 NON-DISCRIMINATION PROVISION

Please Take Notice no covenant, lease, conveyance or other instrument shall be affected or executed by the Borough Council of the Borough of River Edge or by a developer or any of its successors or assigns, whereby land within the redevelopment area is restricted by the Borough Council of the Borough of River Edge, or the developer, upon the basis of race, age, creed, color, handicap, religion, or national origin in the sale, lease, use or occupancy thereof. There shall be no restrictions to occupancy or use of any part of the redevelopment area on the basis of race, age, creed, color, handicap, religion, or national origin.

# SCHEDULE AND DURATION OF THE PLAN

## SECTION 16.0 SCHEDULE AND DURATION OF THE PLAN

### 16.1 SCHEDULE FOR PERFORMANCE

Any Redeveloper Agreement executed for purposes of implementing this Redevelopment Plan shall contain a Schedule for Performance for the construction of the improvements covered by such agreement. In addition to a schedule for commencement of improvements, such agreements shall include a schedule that indicates the approximate time period for property acquisition, development approvals, construction permits, relocation, etc. required prior to construction as well as a projected date for project completion. The Redeveloper Agreement shall also set forth the interim uses for the property and the duration of the period during which such interim uses will be in place.

### 16.2 DURATION OF THE PLAN

The provisions of this Plan specifying the redevelopment of the project area and the requirements and restriction with respect thereto shall be in effect for a period of fifty (50) years from the date of approval of this Plan by the Municipal Council of the Borough of River Edge.

### 16.3 CERTIFICATES OF COMPLETION AND COMPLIANCE

Upon the inspection and verification by the Redevelopment Entity that the redevelopment of a parcel subject to a Redevelopers Agreement has been completed, a Certificate of Completion and Compliance shall be issued to the redeveloper as such parcel shall be deemed no longer in need of redevelopment. At the discretion of the Redevelopment Entity, the redeveloper may submit a final Certificate of Occupancy for approval by the redevelopment entity to serve as the Certificate of Completion.

# RECOMMENDED HORTICULTURAL PRACTICES

# APPENDIX 1.0 RECOMMENDED HORTICULTURAL PRACTICES

## 1.0 Soil Handling and Top Soil

- 1.1 Soil excavated from construction areas shall be used as under-slab compacted fill or removed from the site. Any contaminants found must be remediated.
- 1.2 All areas to be landscaped are to receive topsoil that is friable, fertile natural loam, free of subsoil, stones, roots, noxious plants and extraneous matter to a depth of 3 feet from finished grade as a subsoil cap and new planting root growth zone.
- 1.3 The zone of discontinuity between existing subsoil and top soil should be broken to provide a transition zone if permitted by environmental remediation agreements. Till the subsoil, add several inches of topsoil then till these together before adding the remaining topsoil.
- 1.4 Provide continuous 3-foot deep trenches of high quality topsoil for planting of street trees along streets to widths as specified between municipal curb and sidewalk in lieu of individual tree pits.
- 1.5 Soil depth on rooftop gardens can vary according to plant type. Minimum soil depth for large trees should be 36 inches deep or 6 inches deeper than the root ball; 30 inches deep for small trees, 24 inches deep for shrubs and 12" deep for lawns or groundcover.

## 2.0 Soil Compaction

- 2.1 The deep soil structure of planting areas within parks and street allowances shall be loosely compacted and protected by barriers during construction from heavy compaction by equipment and stockpiling of materials.
- 2.2 Granite cobbles or unit pavers set on an aggregate setting bed shall be placed around street trees where it is necessary to facilitate pedestrian movement directly at the base of each tree.
- 2.3 An expanded slate soil admixture can be added to the sub-base for modular paving over planting trenches between street trees to compensate for compaction and promote root growth.
- 2.4 Where heavy pedestrian circulation is expected, a concrete slab bridging over the continuous planting trench can provide a suitable pedestrian surface while preventing soil compaction in the tree trench. Another acceptable method to extend the root growth area is to use root pipes or drainage mats to provide root paths.
- 2.5 In areas where pedestrian circulation is not conflicting with continuous tree planting trench, provide vegetative ground cover at base of tree and protect planted trench at periphery with low ornamental fences.

## 3.0 Hydrology

- 3.1 All street tree plantings shall have surface watering/fertilizing access pipes and subsurface drainage outlets.
- 3.2 All sodded areas, planting beds and street tree strips shall have in ground irrigation systems.
- 3.3 Site grading and permeable surfaces shall promote maximum return of clean rainwater within parkland, with flat areas graded to 2% minimum. Contaminated surface drainage shall be carried away from landscaped areas.
- 3.4 All plants within rooftop gardens can experience a high evaporation rate due to the drying effects of wind and sun. Irrigation, mulches and moisture-holding soil additives shall be added to help reduce this moisture loss.

## 4.0 Plant Stock

- 4.1 Listed plant species shall be thoroughly searched by a plant broker before consideration of species not listed.
- 4.2 Street trees shall have a branch height of 6 feet minimum at planting.
- 4.3 All street trees shall be of 3.5 inches caliper minimum.
- 4.4 Red Oak shall be dug and planted only during the spring planting season.

## 5.0 Planting and Plant Care

- 5.1 Strategically phase street tree planting to ensure procurement of large quantities of uniform and consistently sized specimens of specifically selected species.
- 5.2 Plant trees only during appropriate Spring and Fall planting seasons to the highest arboricultural industry standards.
- 5.3 All trees shall be guaranteed against defects including death and unsatisfactory growth for a period of two years. All defective plants shall be removed immediately and replaced as soon as weather permits.
- 5.4 Fertilization shall be yearly with a balanced, full spectrum inorganic commercial fertilizer applied at a rate adjusted to remedy deficiencies identified by soil testing reports.
- 5.5 All trees shall be monitored and treated annually by the developer for potential disease or decline in physical condition.

# DESIGN VOCABULARY

## Design Vocabulary – Transit Plaza and Open Space



### Transit Plaza and Open Space

Within the hardscaped transit plaza, varied paving patterns, water features and large plantings are recommended in order to create a pedestrian friendly environment. The inclusion of a multi-use trail within the redevelopment area will help promote alternate modes of travel as well as provide a needed connection to the currently marginalized stream.

## Design Vocabulary – Elevated Sidewalk



### Elevated Sidewalk

The recommended sidewalk mentioned in the redevelopment plan, should create places where people can congregate, while adding a strong landscaped element to the pedestrian realm.

## Design Vocabulary – Building Facades



### Building Facades

The ground floors of the buildings (facades below first stepback) in the redevelopment plan should be masonry construction providing a strong grounding for the structure, with retail shops incorporating awnings into their storefronts. The upper floors (facade above first stepback) should incorporate large sections of glazing in order to make the building seem light and airy.



Galleria

A key element of the redevelopment plan is the galleria. This design feature provides the access to the transit plaza and creates a unique break in the building facade at a major landmark corner



## Parking Decks

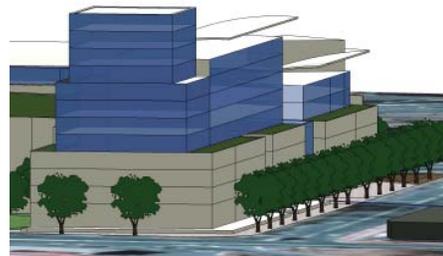
The design of the parking deck is a critical element to the success of this plan. As illustrated in these images the parking structure should not be a blank, exposed structure. The addition of an artificial facade should be adopted for the side facing the transit line. A liner building should be built for the sides of the building facing the street. While a green-faced facade utilizing vines is recommended for the side facing the trail.

## Design Vocabulary – Three-Dimensional Illustrations



The models shown on this page illustrate several key components of the redevelopment plan.

- The design of a strong base to the buildings that feels grounded to the pedestrian realm. While the upper portions of the buildings, designed heavily with glass, feel much softer and transparent.
- The landscaped transit plaza including seat walls, textured paving, trees, vegetation, and water features.
- A key tower that acts as a landmark along Kindekamack Road anchoring the mid-rise buildings adjacent to the site.
- The addition of numerous green roofs that aid in the cleaning and retention of the stormwater that falls on the site. As well as creating additional value for the units that overlook the parking structure.
- A green buffer along the stream edge that contains a multi-use pedestrian trail.

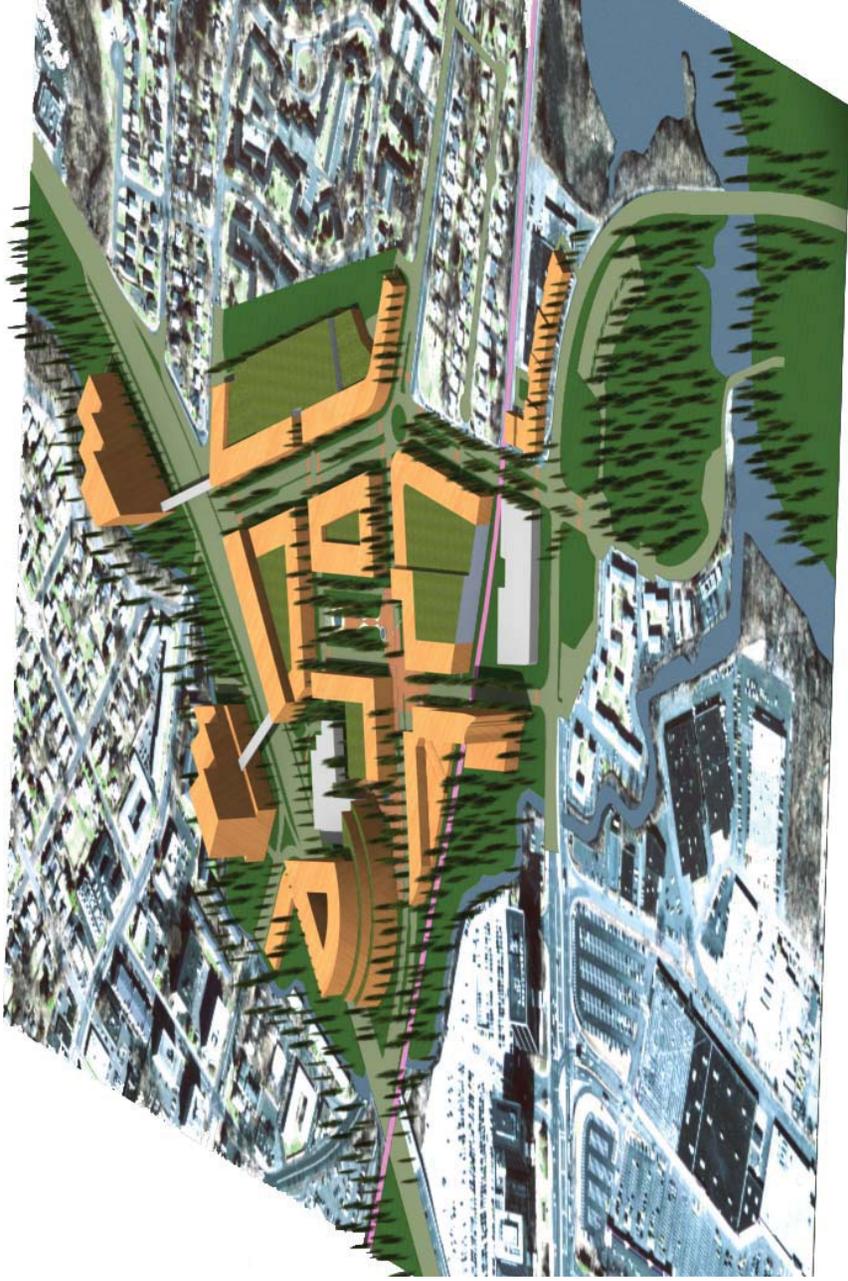


# VISION PLAN: RECOMMENDATIONS FOR A NEW TRANSIT ORIENTED TOWN CENTER BASED ON PUBLIC INPUT

# River Edge, New Jersey

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Recommendations For A New Transit Oriented Town  
—— Center Based On Public Input ——



A. Nelesen Associates, Inc.  
Visioning ♦ Planning ♦ Urban Design

August 1, 2005

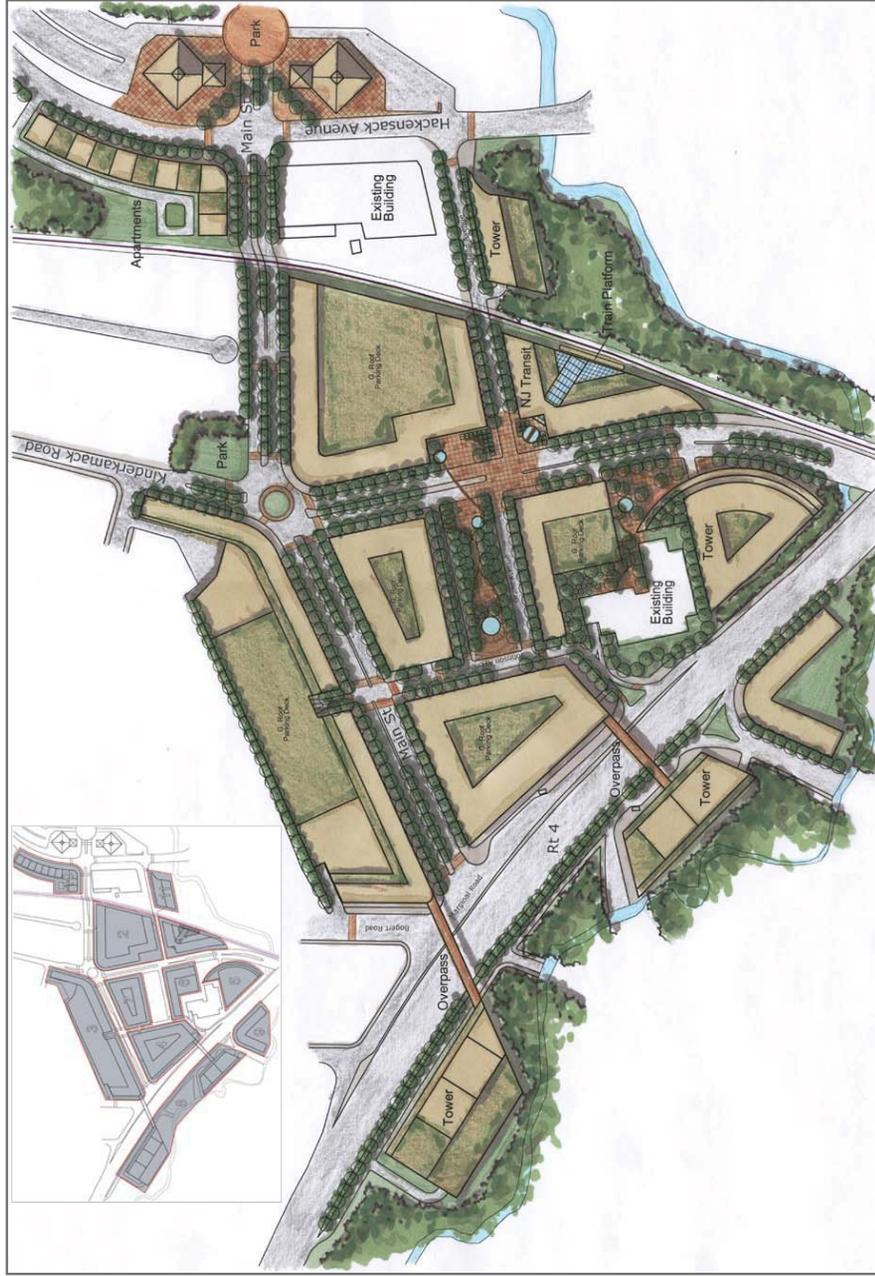
## Existing Conditions



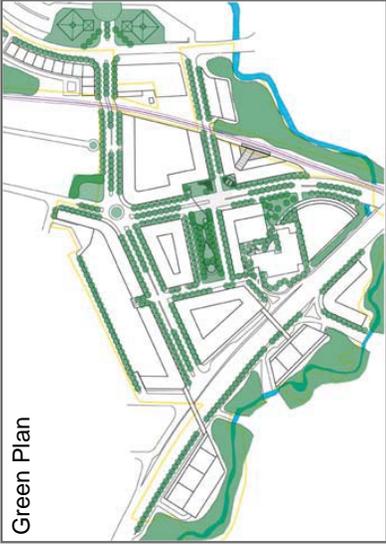
Existing					
Buildings to Replace					
PHASE/PARCEL	Station	Footprint	# Floors	Floor Area	Parking
1	Tracks	-	-	0	168
2	Tracks	25,084	2	50,167	216
3	N of Main	97,111	1	97,111	208
4	W. of Square	14,506	2	29,012	137
5	South Tower	8,045	1.4	11,263	28
6	So. of square	10,276	2	20,552	61
7	N. of square	32,271	2	64,541	266
8	S. of Rt 4	48,647	1.3	63,241	197
9	Auto Dealer	16,787	1	16,787	20
10	E. of Tracks	37,827	1	37,827	63
11	E. Tower	0	0	0	0
<b>Total</b>		<b>290,553</b>		<b>390,501</b>	<b>1098</b>

Dwelling Units/Acre	Existing	
	Net	Gross
Effect on Borough Revenue:		
Equalized Value	\$	72,422,564
Assessed Value	58.24%	\$ 39,404,000
Property Taxes	3.60%	\$ 1,418,544

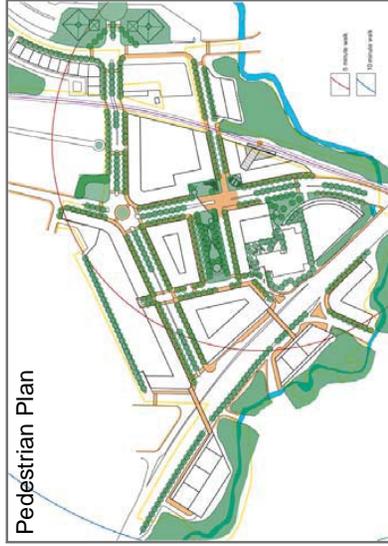
## Proposed Plan



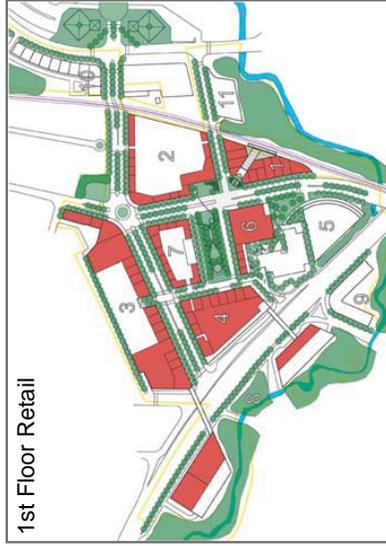
# Program Elements by Floor



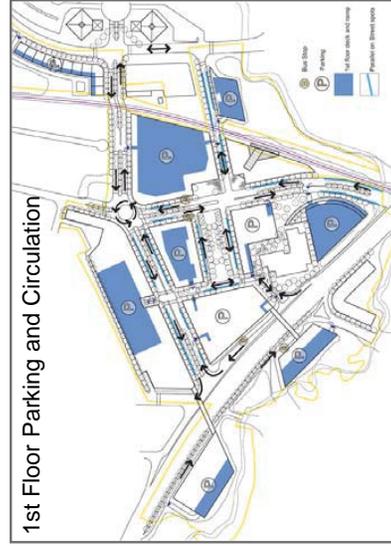
Green Plan



Pedestrian Plan

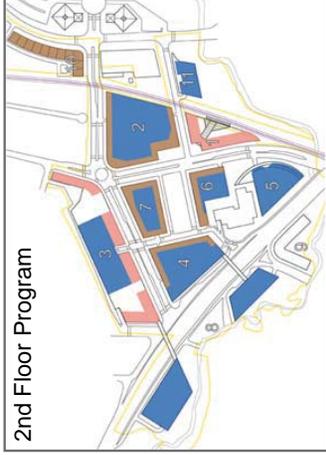


1st Floor Retail

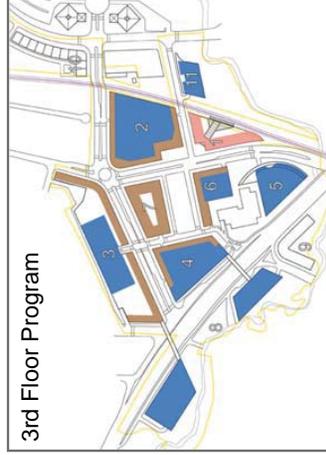


1st Floor Parking and Circulation

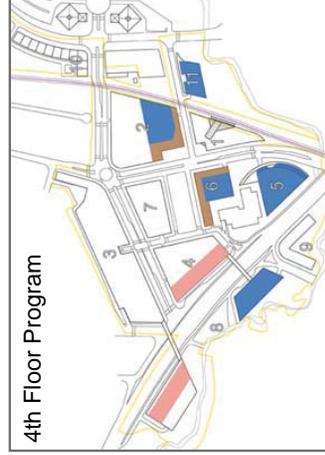
- Retail Space
- Housing Units
- Office Space
- Parking Decks
- Sidewalks & Crosswalks



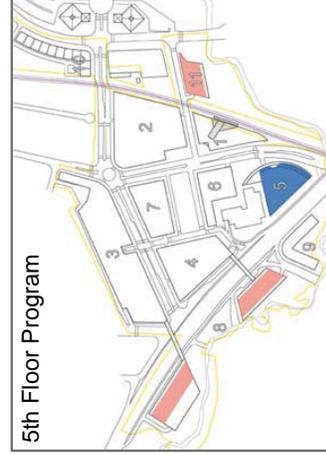
2nd Floor Program



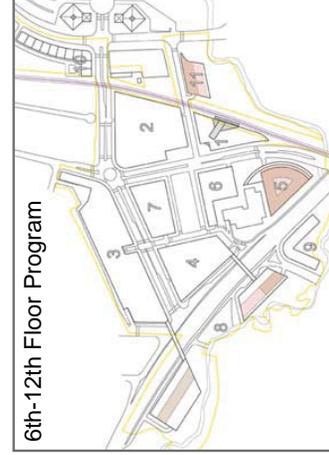
3rd Floor Program



4th Floor Program



5th Floor Program



6th-12th Floor Program

# Proposed Conditions

Proposed Redevelopment															
PHASE/PARCEL	Retail		Office		Residential		Parking		Add'l parallel parking spaces		No. of Floors				
	Area, SF	Area, SF	Area, SF	Units	Area, SF	Units	Spaces provided in proposal	Required Parking**	NJT Parking	Future Parking	Retail	Office	Res.	Parking	Total
1 Station	37,515	58,030					0	0	306		1L	2		3	
2 Trackside	39,747	0	99,368	79	393,160		606	215	300	31	1L	3	4	4	
							1,023	820							
3 N of Main	108,499	56,857	46,214	37	192,120		549	537		41	1B	1	1	3	
4 W. of Square	83,912	60,198	18,103	14	168,602		482	477		22	1B	2	2L	2	5
5 South Tower	0	154,845	173,224	111	240,625		688	617		13	3	4	5	12	
6 S. of Square	39,057	0	49,641	40	74,613		213	169		29	1B	3L	3	4	
7 N. of square	22,337	0	70,668	57	52,789		151	128		32	1L	18	1L	2	3
8 S. of Rt.4	53,040	121,903	128,021	102	260,496		744	672		16	1L	varies	varies	3-4	12
9 Auto Dealer	26,644						80	80			1			1	
10 E. of Tracks		84,600	54,520	44	112,800		322	314		19		2	2	2	1
11 E. Tower							322	314		15		8	4	4	12
<b>Total</b>	<b>410,751</b>	<b>536,432</b>	<b>702,471</b>	<b>509</b>	<b>1,495,205</b>		<b>4,302</b>	<b>3,840</b>	<b>300</b>	<b>246</b>					

Total building area = 3,144,859 SF

Floor Notes:

- L = Liner outside of parking
- B = Includes "Big Box" Retail

\*Future NJT parking allotment would be transferred to Parcel 2

\*\*Parking estimates are based on typical ratios for transit-oriented development

1-10 space per 1,250 sq. ft. residential unit, or 2400 sq. ft. per townhouse

3-20 spaces per sq. ft. 1,000 for offices (except for parcel 3, which has 3 spaces per 1,000 sq. ft.)

3-20 spaces per 1,000 sq. ft. for retail

\*\*\* Total building area includes all residential, retail, office, and structured parking

maximum floors = 12

Dwelling Units/Acre	Proposed	Percent Growth
Net	17.5	
Gross	10.5	
Effect on Borough Revenue:		
Equalized Value (equal to Market Value)	\$ 573,734,495	692%
Assessed Value	\$ 334,142,970	
Property Taxes	\$ 12,029,147	
	3.60%	\$ 10,610,603

# Suggested Redevelopment Area

